

An aerial photograph of the Tumwater Campus area, showing various buildings, parking lots, and roads. A blue vertical bar is on the left side of the image. The text 'The Capital Community: Tumwater Campus' is in the top right, and 'The Master Plan for the Capitol of the State of Washington' is at the bottom center. Faint labels like 'I-5 Freeway', 'Ardmore Way', and 'Lacey Rd' are visible on the map.

The Capital Community: Tumwater Campus

*The Master Plan for the Capitol
of the State of Washington*

TUMWATER CAMPUS PLAN

November 1992

prepared by

NBBJ

for



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General Administration

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PLAN SUMMARY

This document, the master plan for a government and urban center in the Capital Community of Tumwater, is a product of guidelines and directions in the Master Plan for the Capitol of the State of Washington. The Tumwater Campus Plan was formally adopted by the State Capitol Committee in July 1992, after review and approval by the Capitol Campus Design Advisory Committee.

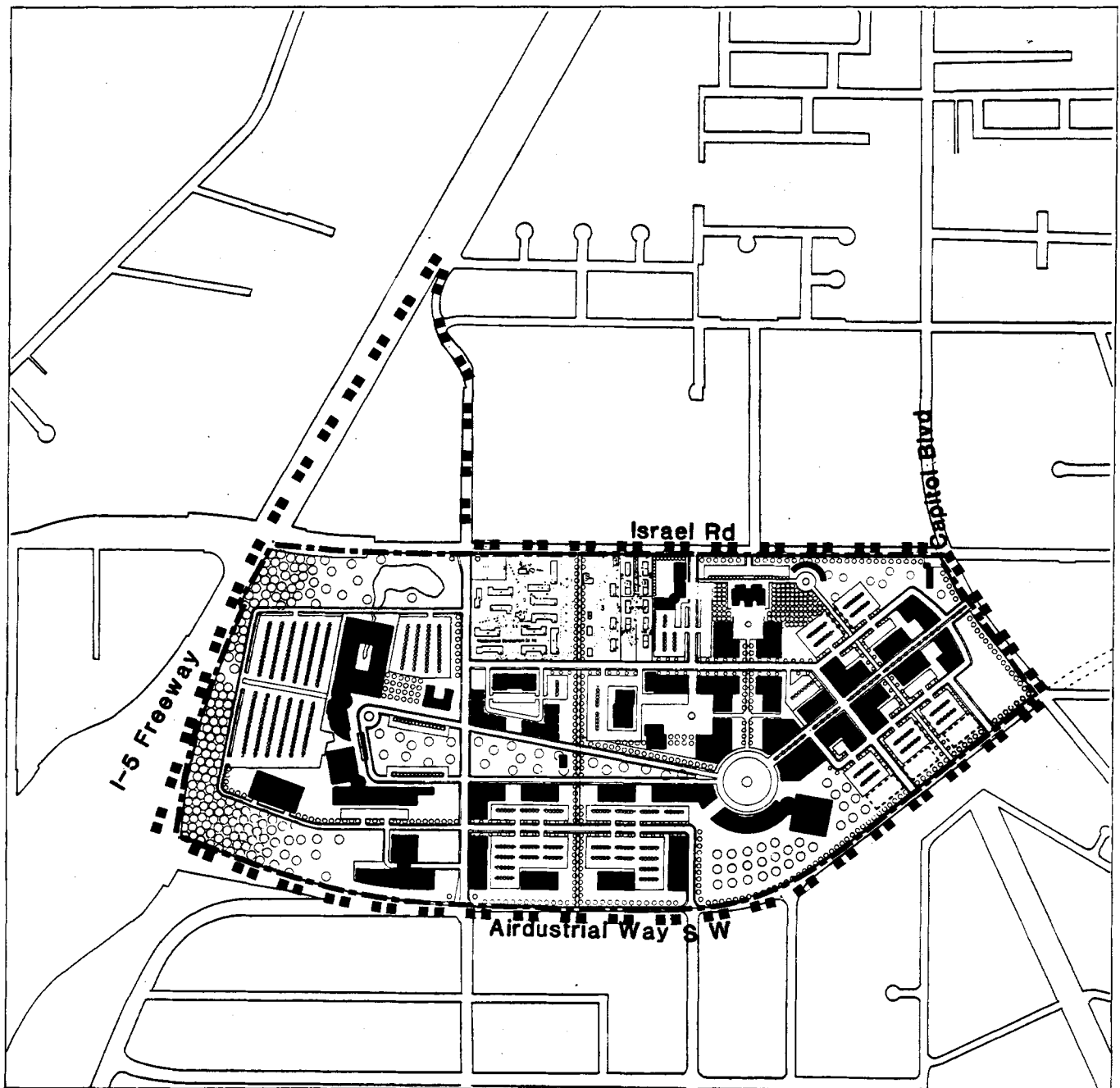
This vision of Tumwater's future downtown was shaped by contributions from a broad range of public and private partners in an intensive planning process. These partners -- the State of Washington, City of Tumwater, Port of Olympia, Tumwater School District, New Market Vocational Skills Center, Intercity Transit Authority, and interested citizens -- will continue to play major roles in the coming decades as this vision of a new, vibrant Tumwater takes shape and matures.

The Tumwater Campus is part of a 190-acre preferred development area, located within the City of Tumwater and bordered by Interstate 5, Airdustrial Way S.W., Capitol Boulevard, and Israel Road*. This plan offers a framework for a mixed-use, urban density, transit-supported community services center, a true downtown for a city whose urban nucleus was decimated by freeway construction in the late 1950s.

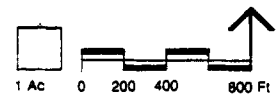
The newly opened Labor & Industries Building on the western edge of the Campus is the first example of the approximately 1.5 million square feet of state offices, private businesses and offices, housing, and community services projected for the Campus by the year 2010. That square footage could nearly double by campus build-out, perhaps 50 years into the future.

The Master Plan for the Capitol of the State of Washington sets out goals, both broad and specific, for development in the Capital Community. It says that satellite campuses, like the vision presented in this document, should not be isolated islands of government but convenient, accessible, and integrated urban centers that both complement the main Capitol Campus in Olympia and stand as active hubs of government and commerce in their own right. In the case of the pioneer city of Tumwater, the master plan calls for creation of a city center and community gathering place; a broad mix of uses; clustered development to create a critical mass for public transportation; and continuing responsiveness to regional goals for growth management and environmental protection.

* The 18.5-acre church property between I-5 and Linderson Way, north of Israel Road, was originally included in the preferred development area. However, as part of the more detailed Tumwater Campus planning, it was determined that this area should not be addressed by the plan. Both existing and planned church development is considered appropriate.



- ■ Preferred Development Area
- Tumwater Campus

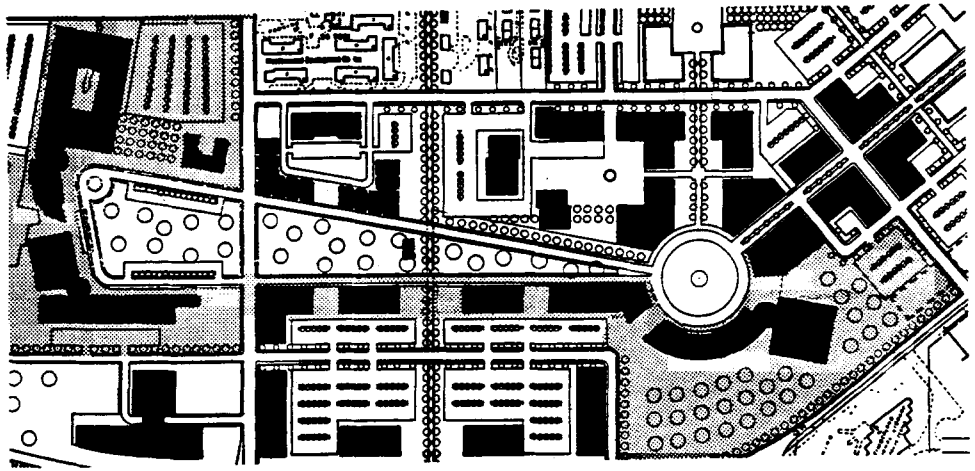


ILLUSTRATIVE CAMPUS PLAN
Tumwater Campus Plan

The plan concept embodies a "Triangle Circle" design idea that has five key ingredients.

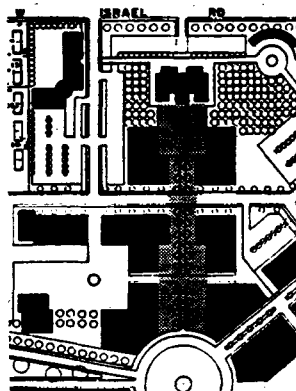
STATE CAMPUS

Development adjoining the central *Triangle* and *Circle* is intended to define these open spaces and create pedestrian-friendly activity centers. State facilities "anchor" the east and west ends of the campus commons. The area south of the *Triangle*, between the two state anchors, may be a mix of uses complementary to the campus or additional state facilities. In any case, development must front the *Triangle* and reinforce its southern edge.



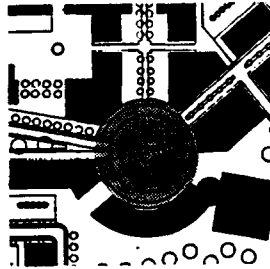
CITY HALL CAMPUS

A corridor open space lined by civic activities is terminated by the city hall. This area welcomes public involvement and is ideal for concentrated community functions. The proposed library is part of the composition.



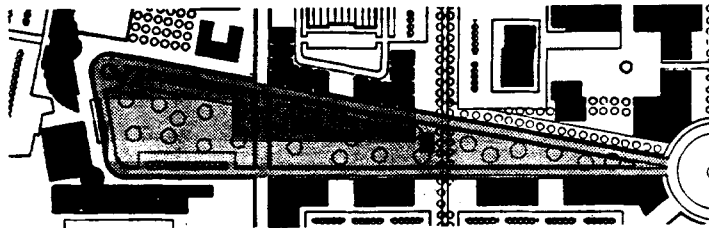
CIVIC CIRCLE

A central open space that provides a civic focus for state, city and private development. This special place is the pivotal "heart" of the campus and a place for gathering and ceremony.



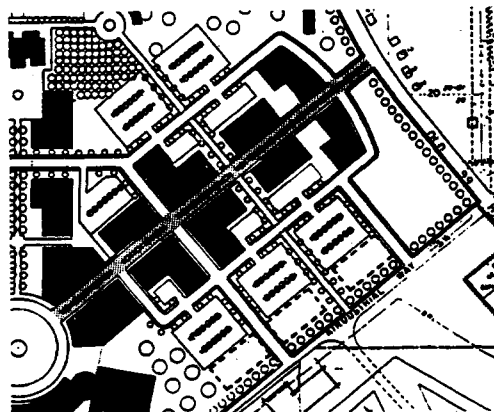
TRIANGLE COMMONS

A naturalistic park open space that preserves trees, defined along its edges by state and private offices and school district facilities. The landscape changes from natural to urban from west to east. This area is vital to unify all parts of the Campus and juxtaposes the natural setting with the urbanizing center.



MAIN STREET

A tree-lined boulevard of retail/commercial activity that is linked with Capitol Boulevard. This place can be a traditional, pedestrian-friendly "downtown." A pattern for linkages with future commercial development east of Capitol Boulevard is also set.



In addition, the plan is distinguished by eight elements: land use, density/scale, vehicular circulation, pedestrian/bicycle system, landscaping/open space, utilities, architectural character, and lighting/signage.

LAND USE

The Tumwater Campus is envisioned to include a vibrant mix of land uses: state facilities; private commercial developments of office, retail, and service businesses; residential; educational; civic services; support facilities/services [i.e. child care]; and public assembly facilities and outdoor spaces.

The western portion of the Campus will have large state facilities, including the completed Phase I of the Department of Labor and Industries (L&I) headquarters building. Those westernmost buildings will front on the major triangular open space as defined by the one-way couplet roadway network. Additional multi-family housing will also be clustered along the Triangle adjacent to the existing residential use.

The eastern portion of the Campus, between the *Civic Circle* and Capitol Boulevard, will also have a mixture of land uses: some state facilities, a mix of private sector offices and first-floor retail/services, and a series of park block open spaces defined by the roadways and the buildings themselves.

The *Civic Circle* will be a major public open space and along the linkage to the existing city hall will set the transition to the mix of State, civic, school district (New Market Vocational Skills Center) and private developments along the northern axis to City Hall. Along and near that axis are expected new city facilities, including a new library. Residential development is encouraged in upper levels of structures near the city hall.

DENSITY & SCALE

As a town center, the Campus will consist of multi-story buildings that will define a new downtown for Tumwater. Landmark state buildings and other structures may be taller, with five to six stories. Height exceptions are allowed for specially located features such as cupolas and clock towers, as long as no airport obstructions are created. There are also major public open spaces that preserve and protect the natural beauty and vegetation present on the property. The density will be greatest along the *Triangle Commons*, around the *Civic Circle*, and along the *Main Street* boulevard that will link the *Civic Circle* with Capitol Boulevard. Such density is needed to accommodate predicted use demands and create the development pattern with the related park block open spaces. Further, the density is desirable to create a critical mass of potential transit ridership.

ARCHITECTURAL
CHARACTER

The Campus buildings are envisioned to be distinctive and attractive, reflective of a new, modern Tumwater. The state's facilities will **not** mimic the grand classical character of the main Capitol Campus in Olympia, but rather will have clear identities of their own. At the same time, state buildings must blend into the Campus to fit with the intended mix of public and private enterprise proposed for the Campus. Street-level development will encourage pedestrian activity and access with business fronts on the walkways and roads. The rich history of the area should also be reflected in the naming of places and streets. For example, commemorative improvements could recognize Capitol Boulevard as part of the Oregon Trail.

LANDSCAPING &
OPEN SPACE

The Campus Plan is based on the concept of *Park Blocks* of open space that are defined by the surrounding buildings, a grid of streets, and direct transit access. Accordingly, the Campus Plan encourages many and varied outdoor activities. Those informal and formal activities include walks, picnics, public assembly, and a full range of exercise activities.

The existing groupings of trees are preserved in the western Campus area because their stature and natural beauty are important parts of Tumwater's fabric. If any old oak trees are identified, they will be preserved. It is envisioned that natural vegetation will similarly be preserved [perhaps thinned], particularly in the major *Triangle Commons*. Open space and plantings will encourage a variety of activities as opposed to formal, "monumental" public spaces. Landscaping forms the boulevard along *Main Street* and defines a Campus edge along *Airdustral Way S.W.* and *Capitol Boulevard*. The *Circle* will balance amenities, landscaping, and paving, and may include features such as sculpture and gazebos.

LIGHTING
& SIGNAGE

Lighting and signage will provide a consistent and distinguishable character to the Campus. Architectural features and focal buildings will define primary destinations and access points. Signage and lighting will add to those features by providing information, orientation, and safety. Of particular importance will be transit signage. Transit shelters and other facilities will be consistent with the Campus character and meet the needs and standards of Intercity Transit. Lighting must not create any navigational hazards for the Olympia Airport.

**VEHICULAR
CIRCULATION**

The primary circulation route is a loop extending one way from Capitol Boulevard west along the *Main Street*, around the *Civic Circle*, and west, and around the *Triangle Commons* back to the *Civic Circle* and Capitol Boulevard. Additional access to the Campus occurs at Linderson Way and at New Market Street, which will be completed along its current right-of-way. It should be noted that direct circulation to the north is limited to Linderson Way to discourage traffic from entering the single-family developments and schools to the north of the Campus.

**PEDESTRIAN/
BICYCLE SYSTEM**

Two pedestrian paths are provided in the Campus Plan: one through the center of the property, and one on the eastern edge leading to City Hall and the new library. A bicycle lane is provided along the *Triangle Commons* as well as along each of the pedestrian paths. In addition, walkways throughout the Campus are intended to be wide and generous to provide pedestrian-friendly access and circulation.

UTILITIES

The Tumwater Campus is located within an urban service area with all utilities available. Electrical power, natural gas, water, storm drainage and sanitary sewer systems will require upgrading and extension to complete the utility service system. The concept for distribution follows existing and new public rights-of-way. Storm water retention/detention basins would be located off-campus on Port of Olympia property to the south. A city water well is proposed to be relocated. Utility improvements are envisioned to be concurrent with proposed campus development.

The 1991 Master Plan for the Capitol of the State of Washington sets forth a vision for the entire capital area. This includes not only the formal Capitol Campus but also the Capital City of Olympia and the Capital Community, identified as the cities of Tumwater and Lacey. The master plan's vision recognizes that it is more appropriate for some agencies to be located outside of the main Campus to provide better public access to their services. Moreover, the main Campus cannot accommodate all of the significant evolution expected in state government employment over the next two decades.

That potential expansion led to the Master Plan for the Capitol of the State of Washington, a 20-year guide to acquisition, development, and construction of property on Campus, in the Capital City of Olympia as well as in Tumwater and Lacey, identified as the Capital Community. The Master Plan is a concept for the 1990s and beyond, with its coordination of government facility needs with adjoining communities through urban redevelopment and creation of the Tumwater and Lacey satellite campuses.

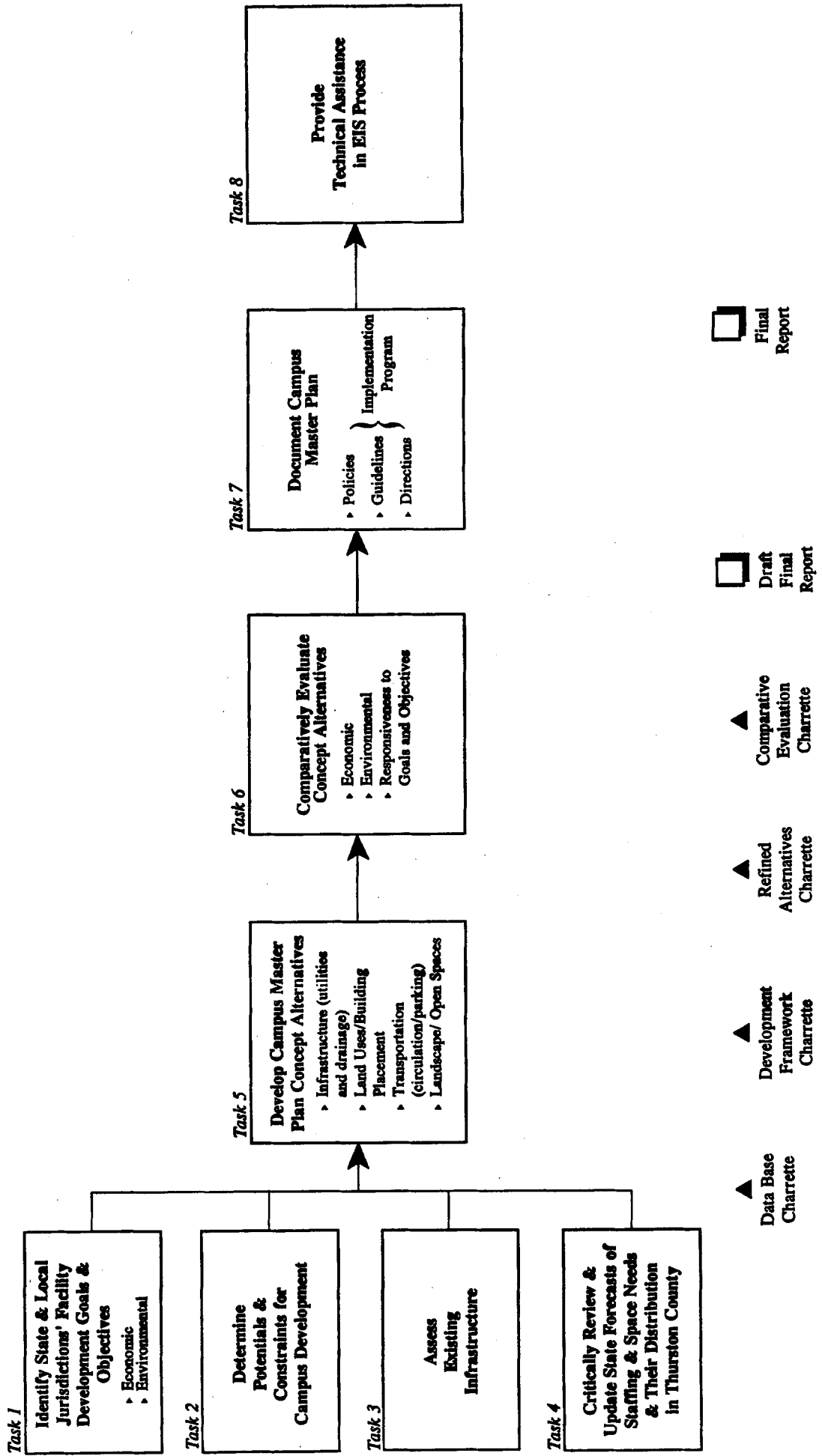
It was against that backdrop that this Tumwater Campus Plan was created.

The challenges in Tumwater were significant.

The 1991 Master Plan for the Capitol of the State of Washington had identified a 190-acre "preferred development area" in Tumwater. One major goal was for the preferred development area to replace the town center lost by the construction of Interstate 5 in the late 1950s. In creating a new Tumwater town center, other goals were to:

- ▶ Site and develop new state facilities to build a "critical mass" of employment, which is critical to encouraging high-capacity public transit and discouraging the use of single-occupant vehicles
- ▶ Help create a mixed-use community center of private and civic public service facilities
- ▶ Create more urban "park block clusters" of open space to preserve the area's natural resources and beauty

PLANNING PROCESS



Achieving those goals involved an eight-step planning process summarized in the graphic on the following page. As indicated, the planning effort involved a number of public work sessions and meetings. The information gathered at those sessions contributed directly to resulting Tumwater Campus master plan. Of similar importance in shaping the plan was the direct and on-going involvement of the active "partners" in the planning effort. The partners included elected officials and senior staff from:

- ▶ The State of Washington's Department of General Administration
- ▶ The City of Tumwater
- ▶ The Port of Olympia
- ▶ The New Market Skills Center/Tumwater School District
- ▶ Intercity Transit

CAMPUS INFLUENCE AREA

The 1991 Master Plan for the Capitol of the State of Washington identified a "preferred development area" for the Tumwater Campus, as shown on the following page. The irregularly shaped 190 acres is bordered on the west by Interstate 5 (I-5), on the south by Airdustrial Way S.W., on the east by Capitol Boulevard, and on the north by Israel Road. The larger study area extending some two miles away was also considered in the planning. The Tumwater Campus area excludes the 18.5-acre church property north of Israel Road. It was determined that existing and planned church development is complementary to the envisioned campus. The Campus area is entirely within incorporated Tumwater, near the southern city limits, and is directly northwest of the Olympia Airport.

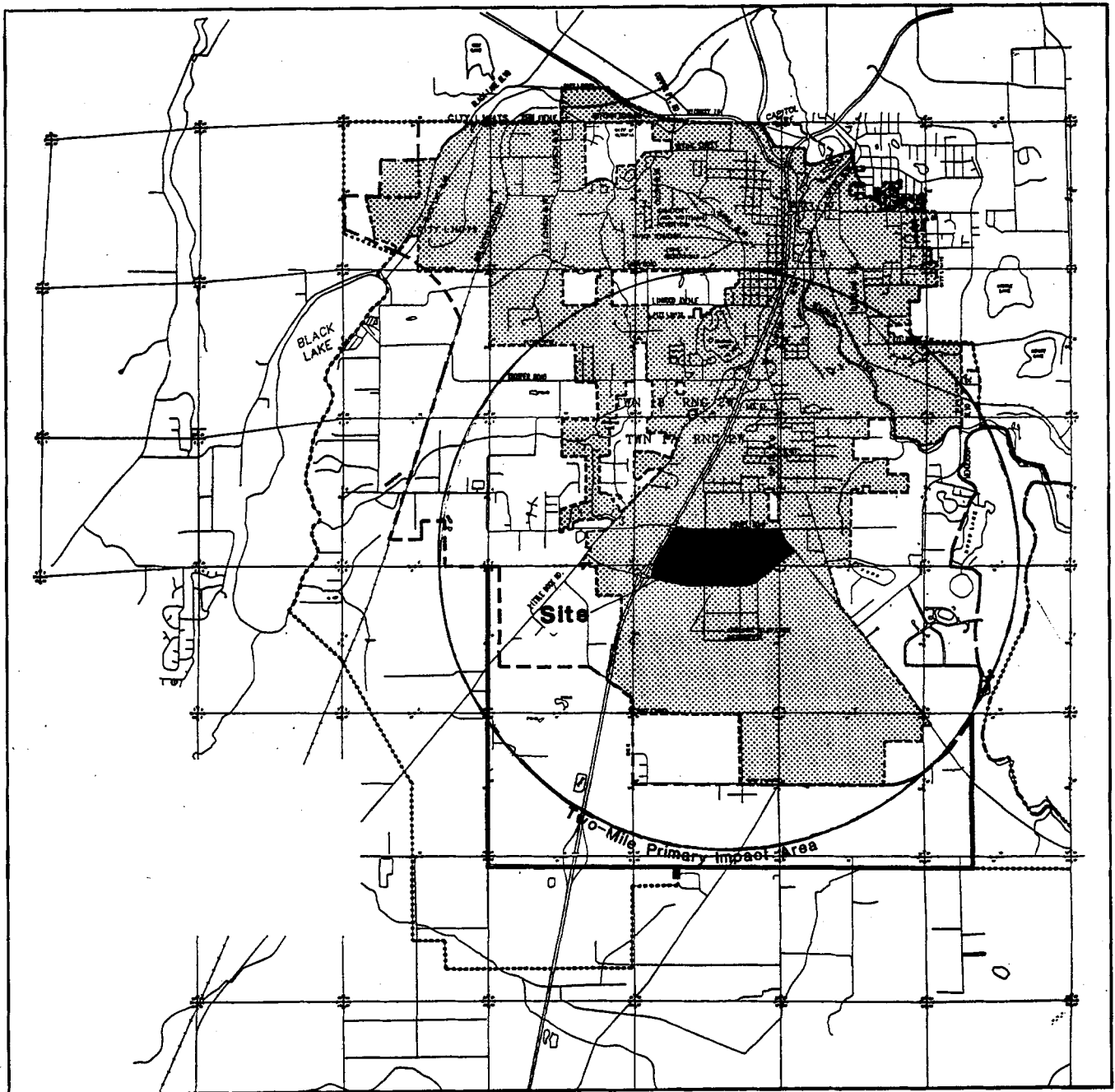
The significant development envisioned for the Campus area will have direct influences on an even larger area. The surrounding land includes a high school and elementary school to the north along with established single-family residential neighborhoods. The areas to the northwest and northeast are largely undeveloped or suburban/rural residential zones. Commercial development is expected to be limited and occur in clusters along major arterials. Port property to the south is envisioned to be more intensively developed with industrial and commercial uses.

Given the established institutional and residential development to the north, the area to be most influenced by the Campus' development includes the I-5 corridor to the west, the commercially zoned property west of I-5, and the Port of Olympia business park area south of the Campus extending to 83rd Avenue S.W., as shown in the Primary Impact Area illustration. Those quadrants will experience comparatively greater commerce flows and development pressures with the maturation of the Campus.

The Campus includes predominantly institutional uses and vacant lands, with a mixture of residential and light manufacturing/distribution uses. The institutional uses are state offices, the new Tumwater City Hall, a vocational skills center, and a church. There is a concentration of multi-family residences along Israel Road. The industrial uses and vacant areas are within the southern portion of the Campus area.

Among the industrial uses within the Campus area is an underground petroleum pipeline and above- and underground petroleum storage tanks. Removal and relocation of these industrial developments will be necessary for the maturation of the Campus.

The subsurface conditions in the area are also significant to the Campus' development. Groundwater is at relatively shallow depths, typically about 10 feet. Surface materials are highly permeable and will require significant storm drainage retention/detention systems as part of the Campus' development.



STUDY AREA
Tumwater Campus Plan

CAMPUS
DEVELOPMENT POTENTIAL

A major goal of the new Tumwater Campus is to create a mixed-use town center of private and civic/public service facilities. In achieving that mixed-use goal, commercial developments must be market-responsive and public facilities must respond to the community's needs.

The public facilities to be located within the town center include state offices and community service and education facilities. The commercial developments envisioned in the town center are private office, retail/services, and housing.

The range of potential market demand (gross building square feet) forecast for the new town center is shown in the following table. The forecast for both public and commercial development is for two time periods: in the year 2010 and at full build-out or maturation of the town center, likely to occur in the first quarter of the next century. Given the uncertainties in estimating demand over such a long time, two demand scenarios are provided for each time period: low and high. The forecast demand is well within the effective holding capacity of the campus, assuming concurrency with infrastructure improvements.

OFFICE

The office demand estimates include approximately one million square feet for state agencies that are candidates to expand and relocate to Tumwater. The private office demand estimates are for major single tenants in the town center (e.g., regional corporate headquarters) as well as smaller office developments occupied by businesses for whom the state is a major client. In total, the office demand estimates range from a low of nearly 1.2 million square feet in 2010 to more than 1.6 million with the full maturation of the Campus.

**COMMUNITY SERVICE
& EDUCATION**

The community service and educational facility estimates reflect a blend of facilities typically found in the town centers of communities of the future size and nature of Tumwater.

RETAIL/SERVICES

The retail/services estimates are based on the numbers of employees and visitors in the town center and their projected expenditures for convenience retail, personal services and specialty retail. The modest amount of facilities for such uses varies from a low of 50,000 gross square feet in 2010 to nearly 200,000 at the Campus' maturity.

RANGE OF DEVELOPMENT (Gross Square Feet)

Land Use	Type of Development	2010			Full Build-Out	
		Low	High	Low	High	
Office	State Office Development	1,000,000	1,000,000	1,000,000	1,000,000	1,000,000
	Private Office Development					
	▶ Single Tenant (Corporate Headquarters)	100,000	200,000	200,000	400,000	400,000
	▶ Multi-Tenant (State supporting)	<u>25,000</u>	<u>50,000</u>	<u>50,000</u>	<u>150,000</u>	<u>150,000</u>
		125,000	250,000	250,000	550,000	550,000
Community Service and Education	City Hall & Related Facilities	<u>90,000</u>	<u>110,000</u>	<u>80,000</u>	<u>100,000</u>	<u>100,000</u>
		1,215,000	1,360,000	1,330,000	1,650,000	1,650,000
	Library, Community Center, Performing Arts Center, Medical/Social Service Clinics, Vocational School	80,000	100,000	100,000	200,000	200,000
Retail/Services	Specialty Retail & Personal Services	25,000	50,000	50,000	100,000	100,000
	Restaurants/Deli's/Markets	15,000	25,000	25,000	40,000	40,000
	Entertainment (theater, skating rink, etc.)	<u>10,000</u>	<u>20,000</u>	<u>20,000</u>	<u>40,000</u>	<u>40,000</u>
		50,000	95,000	95,000	180,000	180,000
Housing	Townhouses (60-unit max.)	120,000	150,000	150,000	150,000	150,000
	Low-Rise MF (150-unit max.)	<u>80,000</u>	<u>140,000</u>	<u>140,000</u>	<u>180,000</u>	<u>180,000</u>
		200,000	290,000	290,000	330,000	330,000
		<u>1,545,000</u>	<u>1,845,000</u>	<u>1,815,000</u>	<u>2,360,000</u>	<u>2,360,000</u>
TOTAL DEVELOPMENT						
Parking	Without TDM					
	▶ Spaces	4,325	5,340	5,340	7,200	7,200
	▶ Square Feet	1,513,750	1,869,000	1,869,000	2,520,000	2,520,000
	With TDM					
	▶ Spaces	3,750	4,730	4,730	6,425	6,425
	▶ Square Feet	1,312,500	1,655,500	1,655,500	2,248,750	2,248,750

The housing estimates reflect a blend of multi-family residences that would be appropriate to the new town center. It should be noted that the estimates are a small fraction of the forecast total demand for housing in Tumwater's future.

In sum, the forecast range of development for the new town center ranges from nearly 1.5 million in 2010 to more than 2.3 million square feet of new development with the town center's completion. Those numbers represent both major changes to Tumwater's fabric and the substance of the design challenge for this master plan.

**DEVELOPMENT
PRECEPTS**

The design intent for the Campus has its origins in the 1991 Master Plan for the Capitol of the State of Washington, which sets forth a vision for the entire Capital Community. From that origin, the following specific design precepts, or guiding goals, were identified early in this planning effort for the Tumwater Campus:

- ▶ Creation of a new town center with a variety of land uses, development forms and street patterns.
- ▶ Urban scale and density and a system of park-block public open spaces defined by surrounding buildings.
- ▶ Creation of development synergies by locating new state facilities in close proximity to one another to achieve a critical mass early in the town center's development.
- ▶ Achieving a pedestrian- and transit-friendly town center with human scaled buildings, weather-protected transit centers and facilities, and increased transit ridership.

OVERVIEW

Pursuit of the design precepts led to active consideration of a wide array of alternative development plans for the new Campus. The resultant master development plan, labeled the *Triangle Circle Plan* for its obvious major forms, is shown in the following illustration.

The concept for the Tumwater Campus is a vision of a public/private partnership that is distinguished by its organized mix of land uses. The basic idea of the *Park Block* is interpreted to create an urban block grid with open spaces defined by buildings and easily served by public transit. A key in applying this idea to Tumwater was scale: the large, existing land platting is reduced in size to a pedestrian-friendly and urban pattern. Community services, diversity of uses, and creation of a new sense of place are intrinsic values. The plan concept provides a structure to unite existing and anticipated future development. It is intended to be flexible to respond to changing conditions yet be clear in its direction. Five key ingredients make up the framework of the concept:

- ▶ *State Campus* - development adjoining the central Triangle and Circle is intended to define these open spaces and create pedestrian-friendly activity centers. State facilities "anchor" the east and west ends of the campus commons. The area south of the Triangle, between the two state anchors, may be a mix of uses complementary to the campus or additional state facilities. In any case, development must front the Triangle and reinforce its southern edge.
- ▶ *City Hall Campus* - a corridor open space lined by civic activities and is terminated by the city hall. This area welcomes the public and anticipates concentrated community functions.
- ▶ *Civic Circle* - a central open space that provides a civic focus for state, city and private improvement. This special place is the pivotal "heart" of the Campus for gathering and ceremony. Improvements balance paved and landscaped surfaces and include features such as sculpture and gazebos.
- ▶ *Triangle Commons* - a naturalistic park open space that preserves trees and is defined along its edges by state and private offices and school district facilities. This area is vital to unify all parts of the Campus and juxtapose the natural setting with the urbanizing center.
- ▶ *Main Street* - a tree-lined boulevard of retail/commercial activity that is linked with Capitol Boulevard. This can be the major ingredient in a traditional, pedestrian-friendly downtown. The street width must be appropriately scaled to the heights of adjoining buildings. Linkages are also created to structure future commercial development across Capitol Boulevard.

The Campus Plan depicts an organization of circulation, open spaces, and buildings. The building sitings shown are intended to be illustrative. Actual development may differ from the diagram. However, specific projects are expected to follow the design principles of the concept and design guidelines.



PLAN ELEMENTS**LAND USE**

The Campus is envisioned to include a mixture of land uses, distributed both horizontally and vertically. Land uses include: state government; private commercial developments with office, retail, and service businesses; civic/community services; public assembly; religious; residential; open space; and parking. Community service institutions and their supporting activities will dominate the new town center.

The land use of the northwest corner of the Campus is as already envisioned in approved plans. The existing church and additional educational, recreational, and senior housing uses are recognized and will be maintained.

A vibrant mixture of activity, with people potentially present 24 hours a day is anticipated. Land uses that reinforce this activity are included.

Open Space

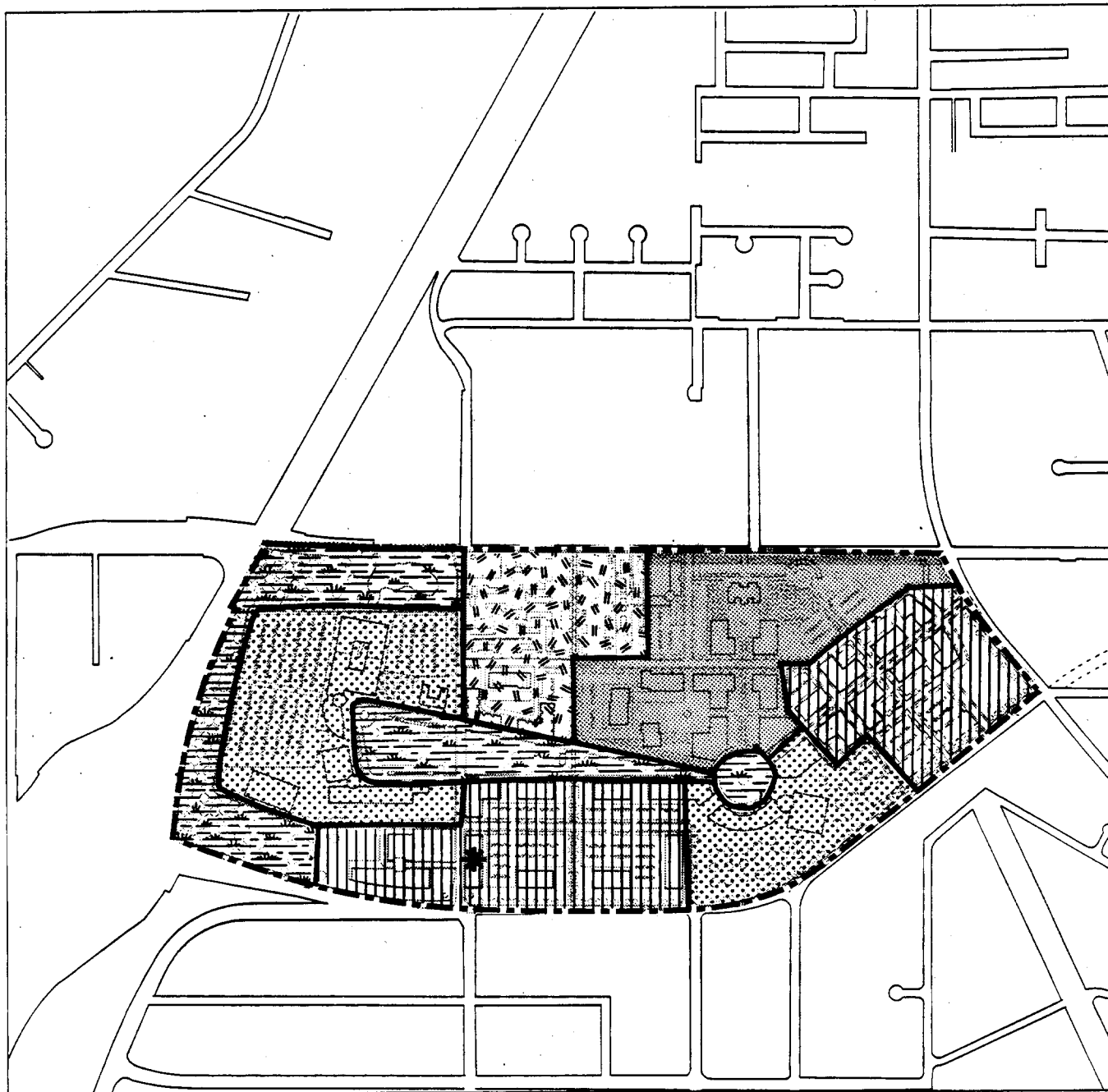
Open space is a significant land use that occurs in the form of both *Park Blocks* integrated with building development and in major concentrations. The park blocks are smaller open spaces defined by adjoining buildings and are described in the Design Guidelines.







The major open space is the naturalistic buffer that rings the western portion of the Campus, adjacent to the freeway and the central *Triangle Circle*. The buffer seeks to preserve and enhance vegetation. The western portion of the triangle also preserves trees and gradually becomes more urban/formal to the east. The circle is the most formal open space. The *Circle* is urban in character, but includes landscaping and other human-scale features.

State Government

State government uses occur in two clusters. One would involve additional state facilities near the existing Labor & Industries Building. A possible land use variation is noted in this area for the parcel at Airdustrial Way S.W. and Linderson Way. This parcel, identified as mixed use, may be changed to state government and/or open space depending on the timing and circumstances related to the tank farm removal; and the initial state development, possibly the Department of Information Services (DIS) Building. If the tank farm is relocated and DIS develops at this location, the state government/open space land use would be expanded.

The second cluster of state government uses is to the southeast of the *Circle*. This area would also include significant landscaping, particularly along the Airdustrial Way S.W. frontage.



-  **Mixed Use**
-  **Open Space**
-  **State Government**
-  **Residential**
-  **Community Service/Civic**
-  **Possible State Government**

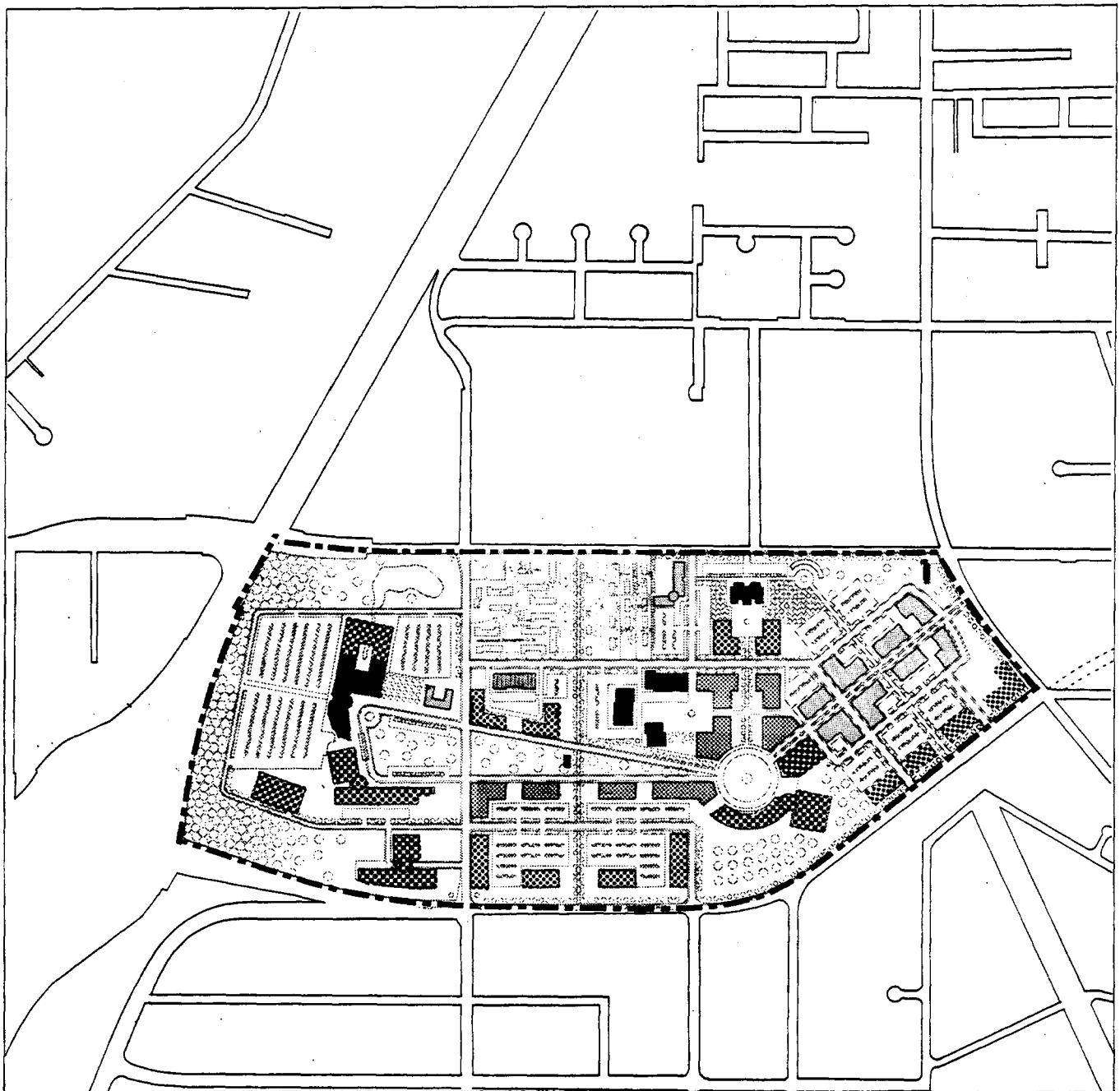







Civic Civic land uses make up a large part of the Campus and are located around the new Tumwater City Hall and New Market Vocational Skills Center. The planned city library is also included in this area. Future additional civic uses, including expanded city and vocational school facilities, are envisioned. To enhance the mixed-use focus, residential development in upper levels may be included in the civic area north of the *Circle*.

Mixed Use Two mixed-use zones are identified in the plan. The eastern area fronting on Capitol Boulevard would become Tumwater's new Town Center, with retail and commercial activities fronting a main boulevard. The pattern would mix street-level retail with upper-level office uses. Parking is located to the rear. A traditional street grid creates a flexible platting pattern where blocks can initially be surface parking but later redeveloped as demand warrants. A second mixed-use area fronts the triangle open space and Airdustrial Way S.W. Buildings are located around the periphery of this zone to establish clear edges to adjoining spaces. The historic Grange Building is located in this area. The plan seeks to preserve and enhance this community asset but envisions relocating it to a more prominent location in the *Triangle* open space. Adjacent retail and commercial uses could be vertically mixed in this zone. Parking and local access roads are central to the zone.

Residential Residential uses are located in a zone that includes existing multi-family development. Additional housing development is planned directly south of the existing residential area. A variety of housing types are encouraged, including townhouses, multiplexes, and more standard multi-family units. While residential development is primarily designated for this area, additional housing is encouraged in the community service/civic area. A deed restriction on port-owned land prohibits more extensive residential uses within the Campus.

Parking Parking is a major supporting land use accommodated within each of the zones. The quantity of parking should reflect City of Tumwater requirements, as well as the long-term reduction in single-passenger vehicle use, as defined in the Transportation Demand Management Act of 1991. Both surface and structured parking are envisioned.



-  Minimum 45' - Maximum 85'
-  Minimum 30' - Maximum 65'
-  Minimum 40' - Maximum 65'
-  Maximum 45'
-  Existing



DENSITY & SCALE

With development of the Campus projected at between 1.5 and 2.3 million square feet, the density of the Campus will be substantially greater than now exists. The plan specifically distributes the development in a pattern that reinforces open spaces and urban "town center" qualities. The scale of structures also contributes to the concept. The planned density and scale of development are illustrated by the placement of structures and their heights. It is not the intent of the plan to create uniformity. Special urban landmarks, such as clock towers or cupolas, should be encouraged. Exceptions to the height guidelines through a design review process should be allowed. No airport navigational hazards should be created.

The tallest structures (minimum of 45 feet/maximum of 85 feet) are state government and a commercial/mixed use project located to establish "anchors" that define the triangle circle open space. These landmarks will be readily distinguishable in the Campus. The next tiers of structure heights include the central portion of the Campus as well as longer-term future expansion at Capitol Boulevard/Airdustrail Way S.W. The first tier of buildings (minimum of 40 feet/maximum of 65 feet) is intended to define the southern edge of the Triangle and northern edge of the Circle. These areas require a minimum density to define the adjoining spaces. The second tier of buildings must have a minimum height of 30 feet but can also extend to the 65-foot height. The building scales contribute to the definition of open spaces and Campus edges. Lower-scaled buildings (at a maximum of 45 feet) are primarily concentrated in the new commercial area in the eastern portion of the Campus. This development is grouped to form the *Main Street*, with a consistently-scaled street volume. Additionally, lower-scaled buildings are identified for the proposed library, day care, and townhouse developments.

**ARCHITECTURAL
CHARACTER**

Campus buildings are envisioned to be distinctive, reflecting a new, modern Tumwater. Campus buildings are not intended to be "monumental"; they should, rather, work within the concept of mixed-use community development. The state's facilities will not mimic the grand classic character of the main Olympia Campus, but will have clear identities that blend into the town center as an intended mix of public- and private-sector development. To that end, street-level development will encourage pedestrian circulation and access by fronting on walkways and roads. Other human-scale improvements will contribute to a development character that contributes to the pedestrian environment.

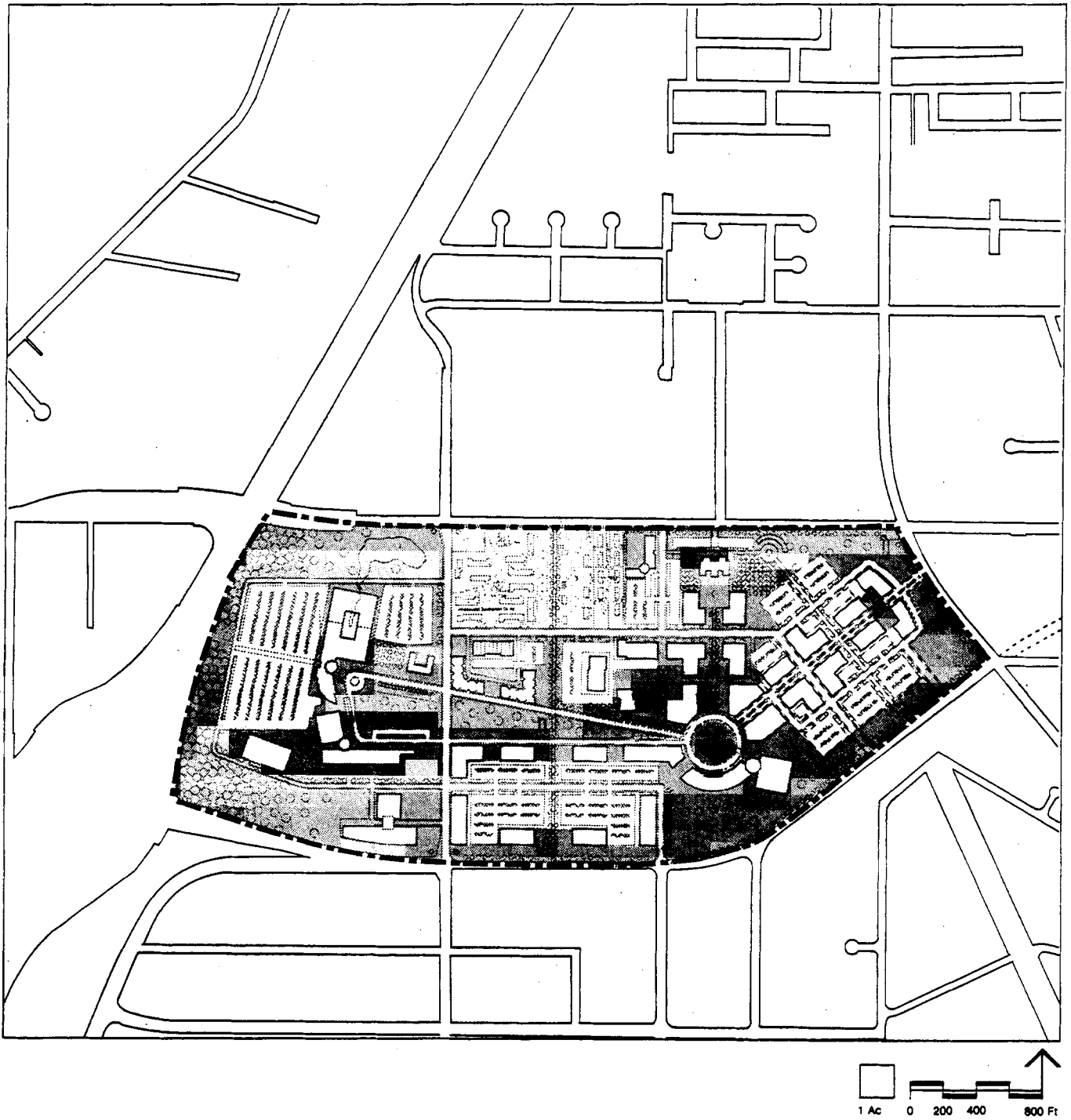
Building designs should be reviewed in terms of each individual project's requirements and as contributors to the collective Campus development. This latter quality is most important for unification of Campus open spaces and creation of a "common thread" to bind development together. Diversity in architectural character is recognized as important. However, a balance is envisioned in the plan that expresses individual and Campus design values.

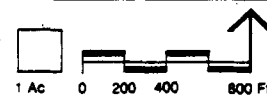
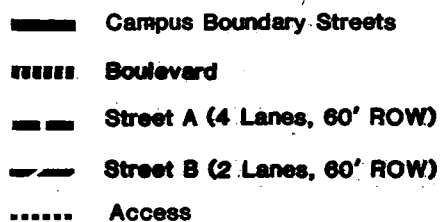
Building materials, color, textures, finishes, and facade articulation, fenestration, and overall building form should reflect Northwest regional design attributes. Creation of an urban character (not rural and not suburban) is key. Buildings should front directly along primary streets. Overhangs and awnings could provide pedestrian protection and link individual buildings. Parking should not disrupt building activity and should be located unobtrusively.

The rich history of the Tumwater area should be reflected in the naming of places and streets. Capitol Boulevard was a part of the Oregon Trail and commemorative improvements should recognize historical aspects.



VIEW NORTH FROM "CIVIC MALL" TO TUMWATER CITY HALL





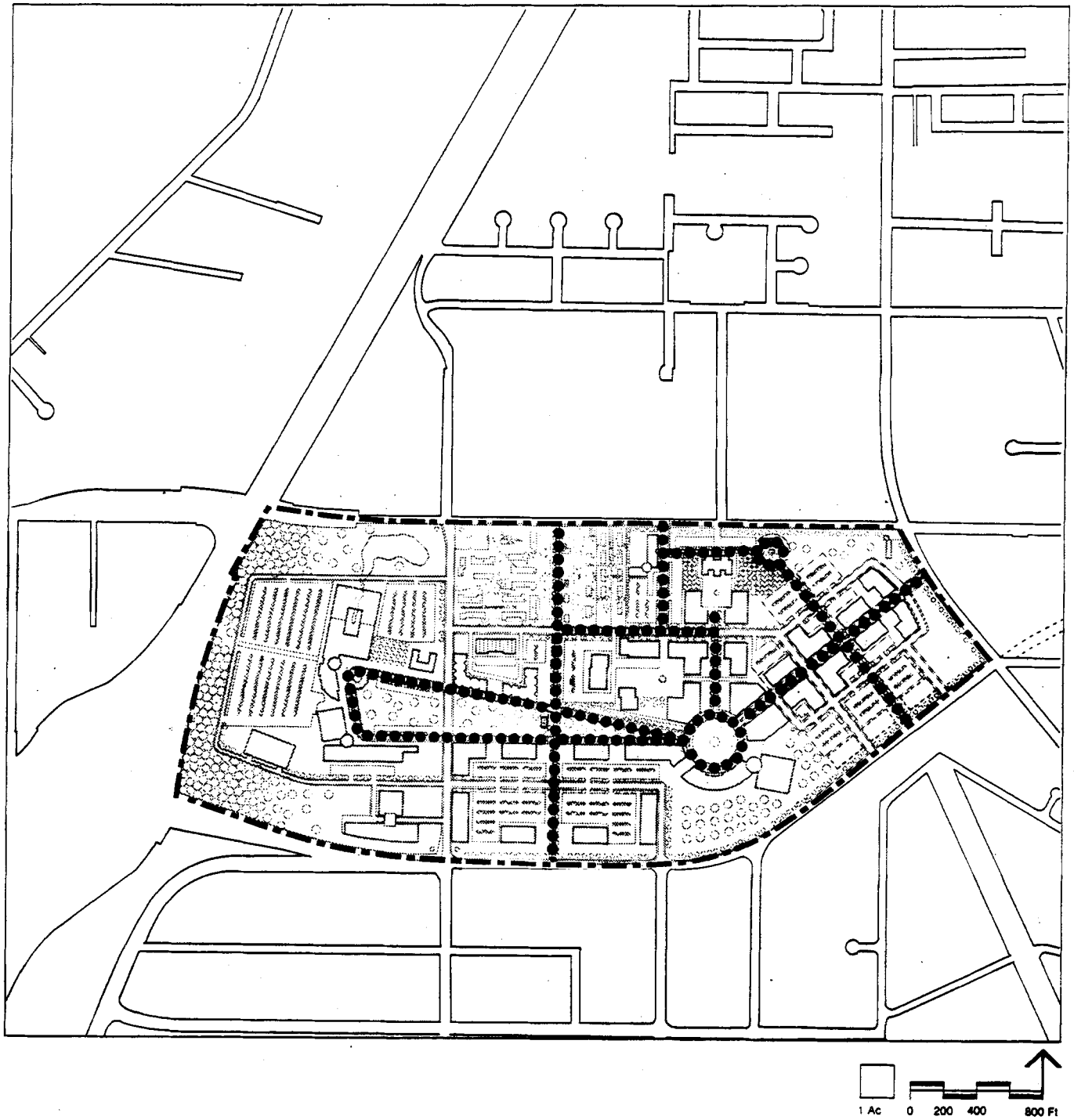
**VEHICULAR
CIRCULATION**

Circulation routes are currently limited within the Campus area. The area is generally structured by the primary streets along the boundaries of the Campus: Interstate 5, Airdustrial Way S.W., Israel Road, and Linderson Way. Their alignment, and the resulting land platting grid, is quite varied. The proposed vehicle circulation system seeks to resolve the juxtaposition of the varied street patterns and establish a clear street hierarchy.

The function of the "boundary streets" is not expected to be changed by the Tumwater Campus Plan. Interstate 5, with its full interchange at Airdustrial Way S.W., will continue as the major north/south through route and Campus access/egress point. Capitol Boulevard will also continue as a major north/south arterial with direct access to other state facilities in Olympia, the airport to the south, and highway-oriented commercial activities. Airdustrial Way S.W. is a major east/west arterial and, with its planned extension to the east, will become an even more important arterial. Israel Road is also a key east/west arterial. However, the plan seeks to de-emphasize its use by limiting connecting streets. High traffic on along Israel Road are not compatible with the schools and residential development along the road.

Vehicle circulation within the Campus includes a boulevard, two types of streets (Street A - four lanes, 60-foot right-of-way; Street B - two lanes, 60-foot right-of-way), and access roads. The highest capacity street is Linderson Way, which has four lanes. This route is a major one for access to I-5/Airdustrial Way S.W. as well as linkages to the north. Other two-lane streets are shown in the plan and include a series of connecting routes serving Campus development. The Plan's feature road is the central boulevard that links Capitol Boulevard with the *Circle* and then borders the *Triangle* open space. This boulevard has a tree-lined center median in the eastern portion of the Campus and follows a counterclockwise one-way traffic pattern around the *Circle* and *Triangle*. Finally, access roads complete the street hierarchy by providing local routes to parking and services.

The incorporation of transit service is an important part of the plan concept. The street hierarchy provides one routing basis with major activity centers (high employment concentrations) located along major streets. Both public transit and shuttle service can be accommodated in the plan.



PRIMARY PEDESTRIAN/BIKE PATHS
Tumwater Campus Plan

UTILITIES

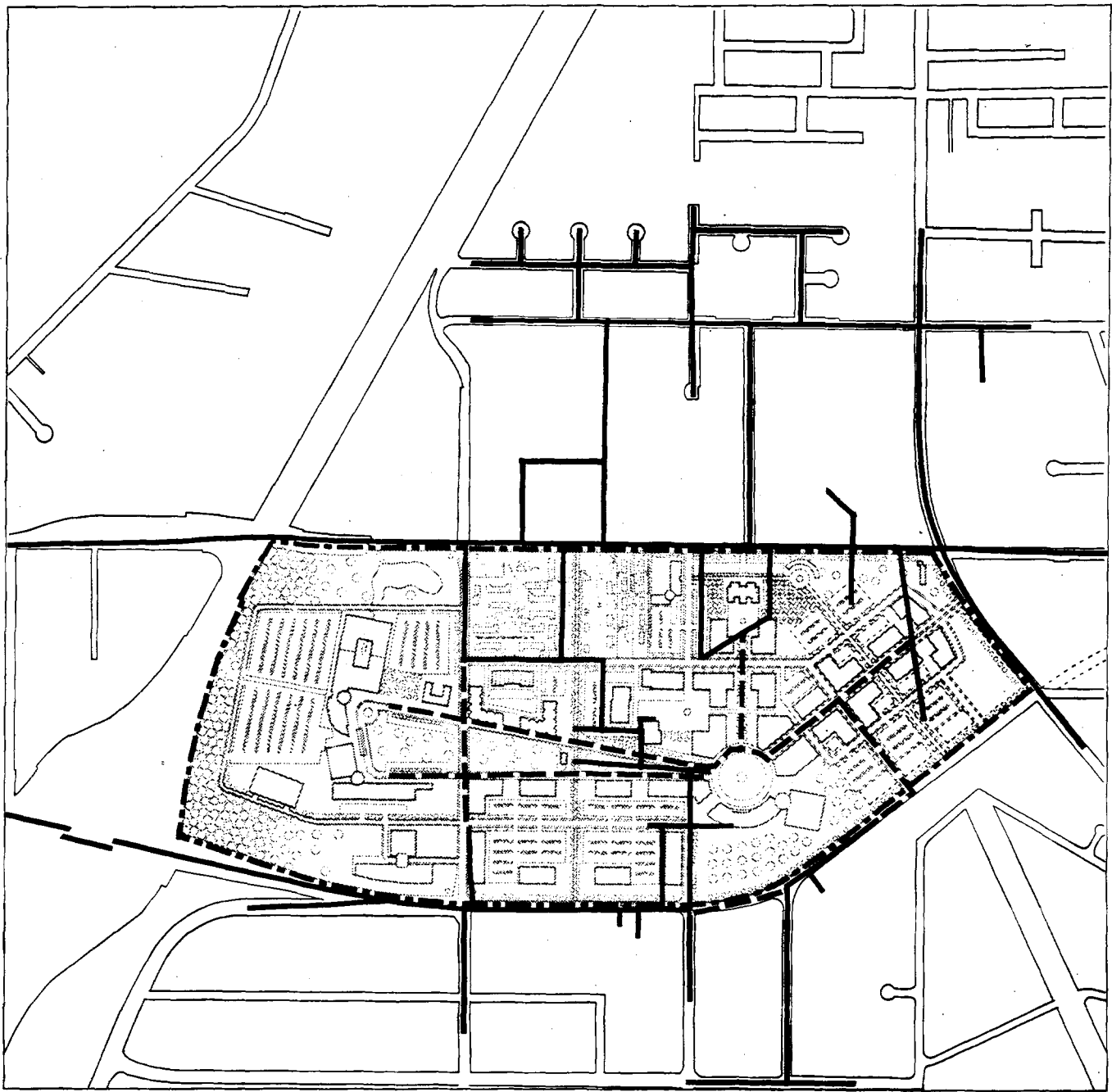
The utility infrastructure is a basic functional requirement of the Campus Plan. Systems are expanded and upgraded to serve the envisioned Campus development. The area is already within an urban utility service district and most systems are in place. The concept for distribution lines is to locate utility improvements within the public street rights-of-way, both existing and new. Utility improvements must be in place to allow service to development when it is occupied. Concurrency requirements of the Growth Management Act must be met. It is expected that the identified utility needs of the Tumwater Campus will be considered and coordinated with on-going planning for a much larger area. A comprehensive capital facilities plan, with participation by all affected, jurisdictions should be prepared. The Tumwater Campus infrastructure is one part of the larger systems.

Electrical

Main electrical power feeds are already located along Airdustrial Way S.W. and Linderson Way. Future electrical connections are expected to be placed underground within the Campus. A new substation has been identified as a future need, probably located along Israel Road to the west of Interstate 5. While dual power feeds are not available, there is adequate existing electrical power.

Natural Gas

Natural gas lines include 2-inch, 4-inch, and 6-inch mains currently serving the Campus. Future improvements could be provided by the supplier and would be located within street rights-of-way.

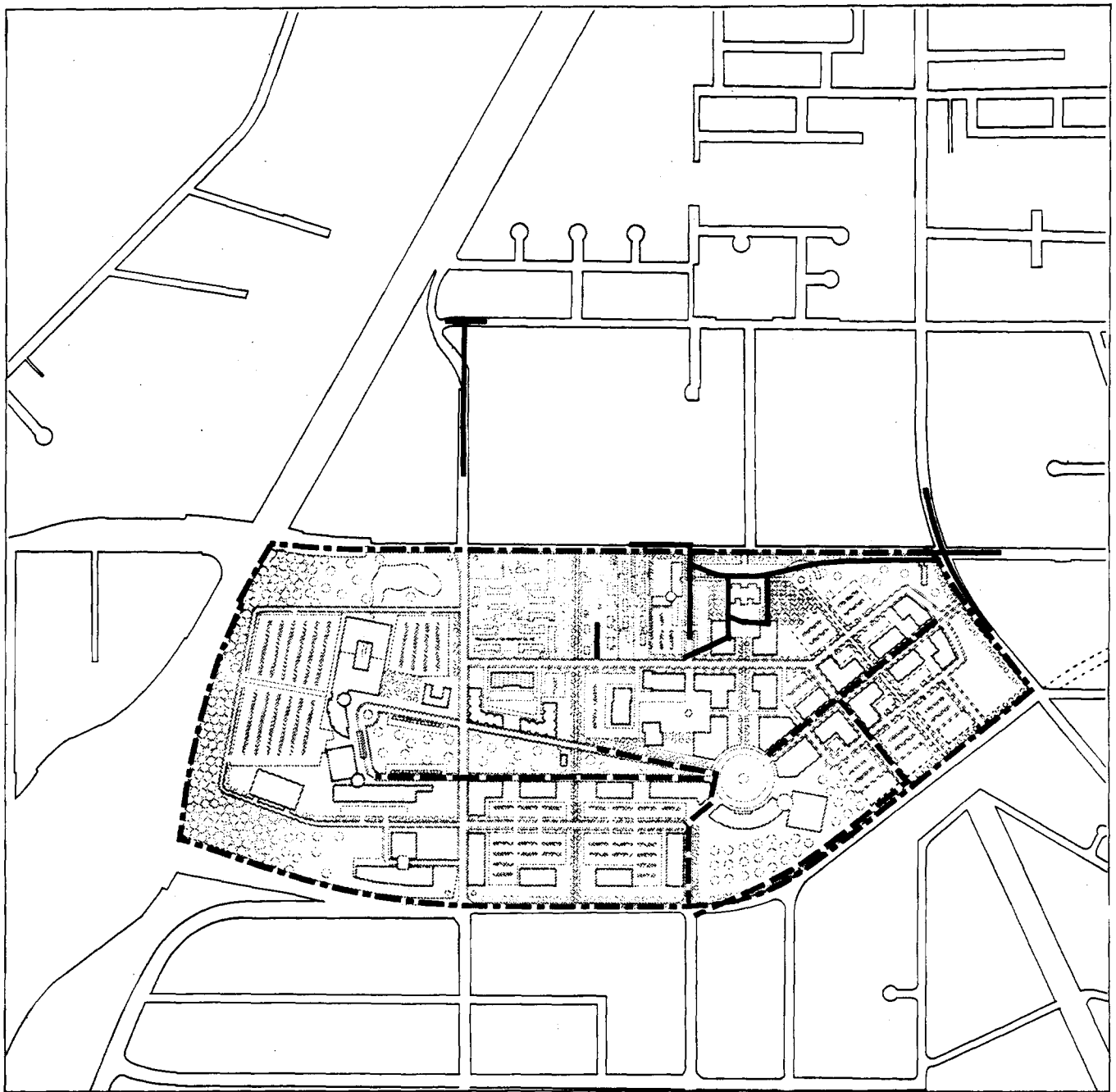


— Existing Water System
- - Proposed Water System



Water

Potable water distribution mains include the City of Tumwater system and its recently acquired Port of Olympia distribution system. Mains generally follow street rights-of-way. Extensions could occur to serve the development. It is recognized that an existing well (Well #15) must be relocated. A replacement water supply source should be developed.

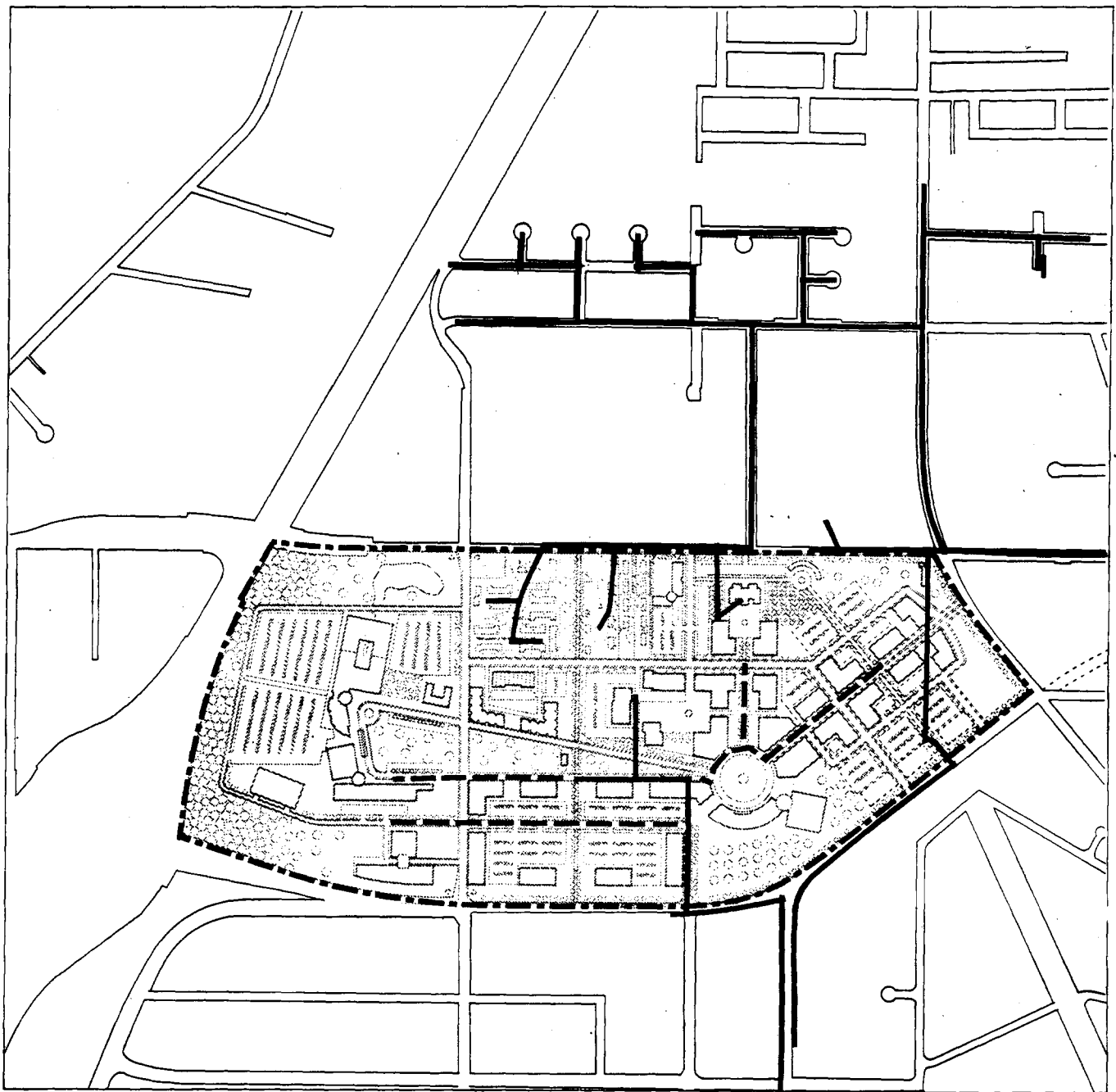


- Existing Storm Drainage System
- - Proposed Storm Drainage System



Storm Drainage

Storm drainage systems are critical to protect the aquifer. Drainage lines would be extended to the south to Port of Olympia property. Here a retention/detention facility would be developed. The drainage collector system would have to be developed with Campus building projects.



SEWER SYSTEM
Tumwater Campus Plan

NBBJ

Sanitary Sewer

Sanitary sewers serve the Campus and were recently upgraded with the Valley Interceptor and Terminal Street Pump Station. There appears to be adequate capacity to accommodate the planned development. System connections/extensions to projects will be required.

DESIGN GUIDELINES

The design quality of the Tumwater Campus must reflect the vision for its mixed-use, town center, community-service functions. Design guidelines offer a basis for design review decisions that implement the Campus Plan concept and encourage better and more consistent site planning and building design. They incorporate flexibility to meet Campus design objectives while responding to changing conditions. Finally, the guidelines facilitate communications among all principal interests to mitigate problems and find innovative development solutions.

While the Concept Plan for the Campus describes a Campus vision, the following design guidelines provide more detail for the realization of this vision. The Campus will be realized as the sum of a potentially wide range of interests and conditions. These varied interests likely will not create identical visions of how the plan should be carried out. Rather, through the implementation of design guidelines, significant and universal qualities of the plan can be established.

The design guidelines are divided into categories: buildings, landscape, roads, and transit.

- ▶ **Buildings.** Building development should promote diversity in individual projects, yet respect adjacent structures. Special spaces are intended to be created by grouping buildings in a number of different ways. Buildings should be designed to be a minimum of 30 percent more energy efficient than required by the state energy code. Water conservation measures should also be incorporated in buildings and landscaped areas.
- ▶ **Landscape.** The landscape of the Campus is a critical organizing ingredient, ranging from the natural to the urban.
- ▶ **Roads.** Safety and accessibility are basic requirements. Encouraging improved transit use is most critical. The circulation hierarchy is directly linked with land use and density to create a distinct Campus pattern.
- ▶ **Transit.** Transit is specifically addressed in the guidelines. Alternatives to the single-occupant car must be planned for and are in fact key design elements in the plan.

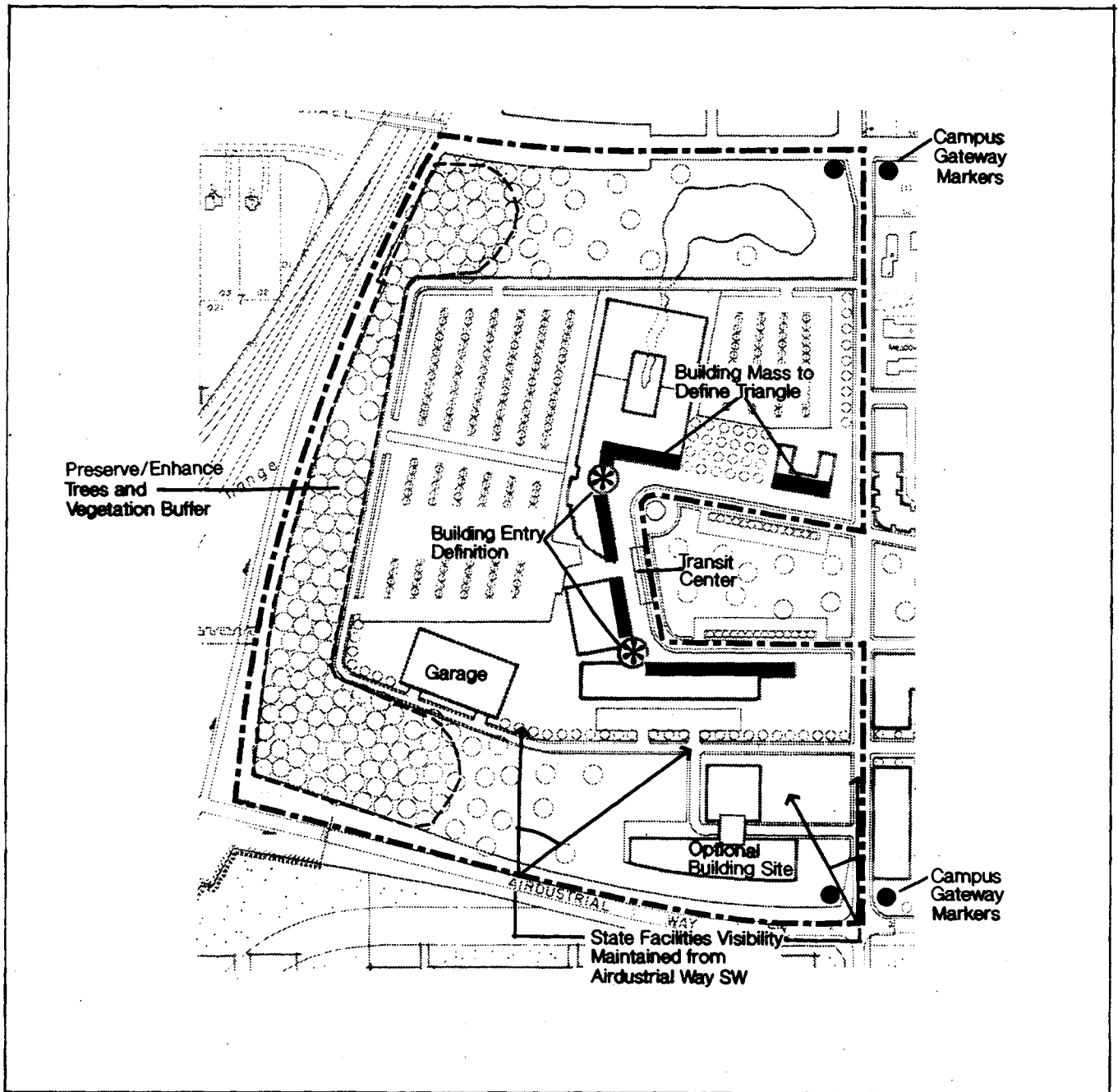


DEFINITION OF AREAS
Tumwater Campus Plan

For each guideline category, the design intent is described in text and graphics. Each category is further divided to focus on individual locations within the Turnwater Campus, such as the state building sites or the Triangle open space or the boulevard road system.

In general, the design guidelines are intended to assure the orderly development of private and public mixed uses in order to create a special urban place for people.

Seven different areas within the Campus are defined to guide building development. Each area is unique in the way buildings are envisioned to be developed. The following section explains the design intent for each area and details guidelines for use, building placement, structure height, parking, and overall character. A graphic highlights the key design elements for each building area. All guidelines, when applied to individual projects, are subject to design review by the Capitol Campus Design Advisory Committee. Note that exceptions to the guidelines may be allowed. For example, height limits may be exceeded for special landmark features, such as clock towers or cupolas.



AREA 1

INTENT The design goals of this area are to appropriately distinguish key state facilities, to "anchor" the western end of the *Triangle* open space, and to provide visibility and an attractive face for the Tumwater Campus from Interstate 5.

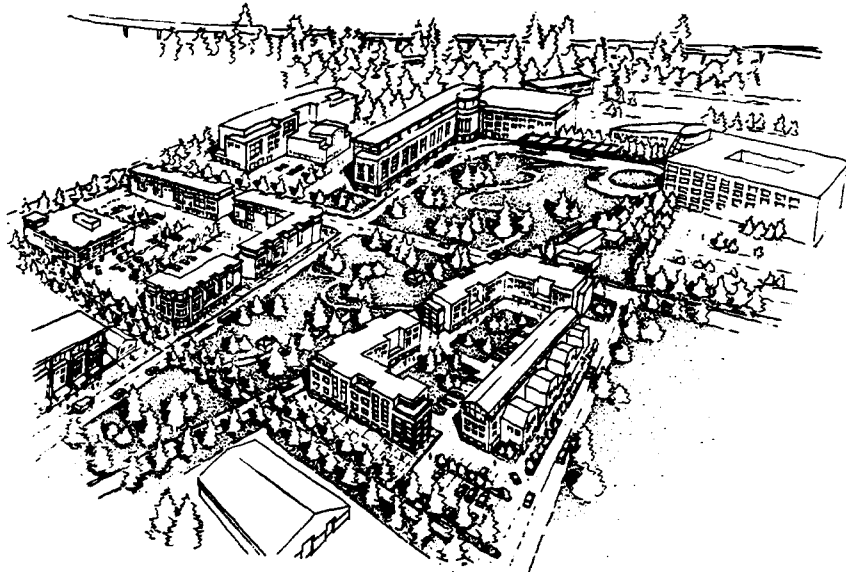
USE State facilities, and a contingent private use, as discussed below.

PLACEMENT Area 1 surrounds the west end of the *Triangle* and building development is critical in determining its spatial quality. Setbacks of buildings from the *Triangle* roadway edge shall be a maximum of ten feet. A variation to building siting is shown at Airdustrial Way S.W. and Linderson Way, which is dependent upon the timing of the tank farm relocation, and requirements of the initial state building, which is likely to be DIS. If the state facility has less site need, then a private or another state structure may occupy the site to the south. The siting and program of this additional structure shall be subject to Capitol Campus Design Advisory Committee review. Placement shall be such that these buildings provide significant enclosure and definition of the edge of the *Triangle* open space. Their siting should also enable visibility of the building fronting the *Triangle* from Airdustrial Way S.W. The Child Development Center shall occupy space on the northwest corner of Linderson Way and the northern *Triangle* roadway. Building elevations shall front no less than eighty percent of the property line facing the *Triangle*. Parking structures shall be placed between the occupied buildings and the boundary road buffer. No vehicular access shall occur from the *Triangle* between the buildings. Spaces between buildings should be made such that they can support a variety of public uses, such as park or garden space, plazas or courtyards.

HEIGHT Building heights for Area 1 shall be a minimum of 45 feet and a maximum of 85 feet.

AREA 1**CHARACTER**

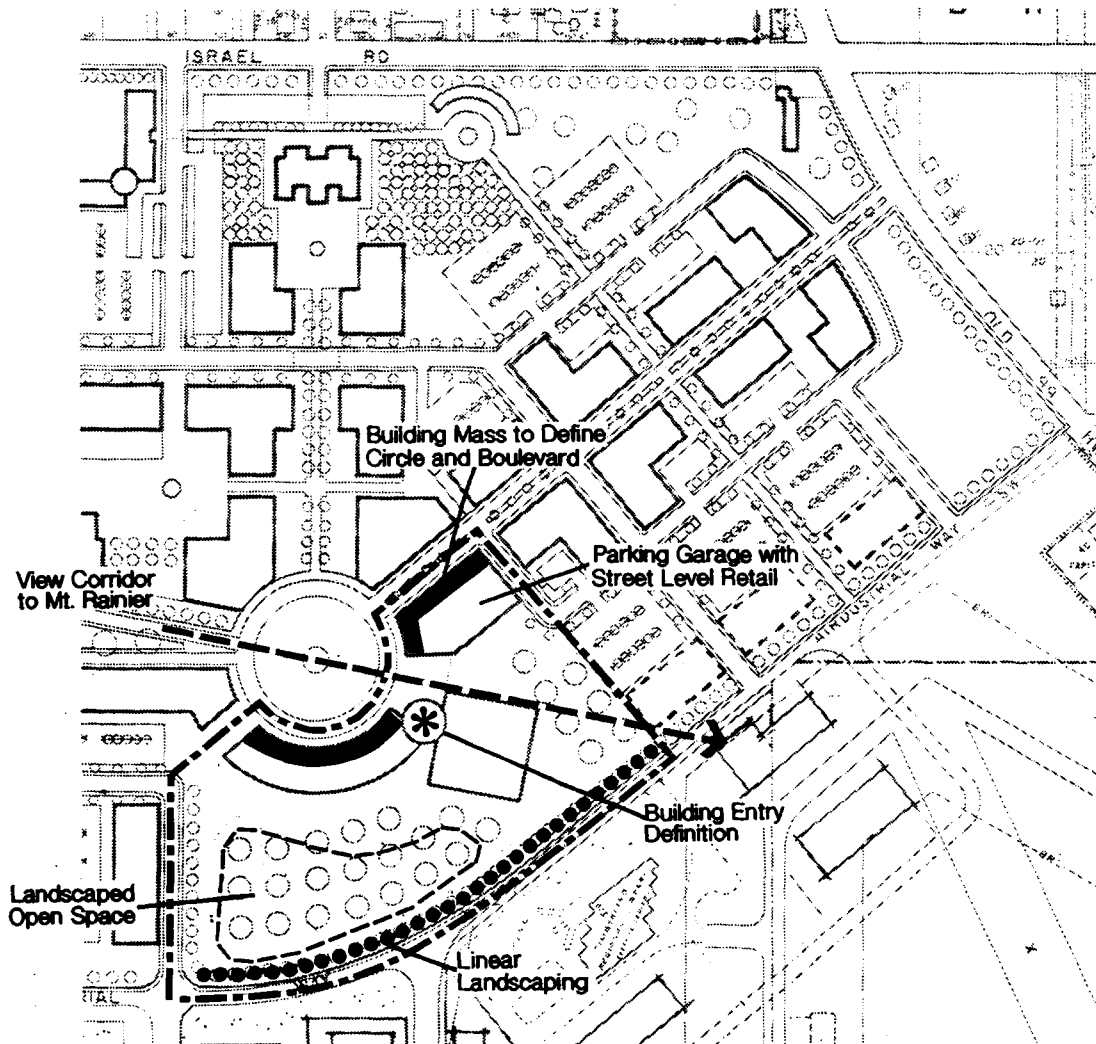
Facades onto the *Triangle* shall promote an urban character. The building facade of the ground floor within view of the *Triangle* shall include windows and overhead protection. The use of blank walls (such as concrete cinder block) without facade ornamentation is not permitted. Building materials such as brick, masonry, glass, tile, stone, or wood is required on the building facade. Uninterrupted glass curtain walls and mirrored glass are not permitted.



VIEW SOUTHWEST FROM THE VOCATIONAL SKILLS CENTER TO STATE BUILDINGS

AREA 1**PARKING**

Structured parking is required to accommodate the majority of additional parking needs in this area. Existing surface parking will remain. A new garage site is identified. Design of small areas of parking, separated by landscaped visual barriers, is encouraged. Design of large, unbroken expanses of parking shall be avoided. A minimum of 10 percent of parking areas shall be devoted to plantings and landscaping, which shall not be limited to the perimeter of the parking areas. Parking structures shall be effectively buffered from view from the *Triangle*. New state facilities shall provide structured parking to be located to the rear of any new buildings. Because existing parking will be displaced with the creation of the *Triangle*, a maximum of 30 short-term visitor parking spaces will be provided within the *Triangle*. These spaces are intended to be readily accessible to the front doors of the two state facilities. These spaces will be located along the perimeter of the *Triangle* in two areas: just south of the Child Development Center; and directly south of that, across the *Triangle*. These parking areas shall be screened from view from both within and from outside the *Triangle*. The screen will be vegetative, and will enhance the form of the *Triangle* along its edge through use of medium to large trees. Additional short-term parking is located to the east of Labor and Industries. Longer-term parking (i.e. for employees) is located to the west, both in surface lots and the proposed garage.



AREA 2

INTENT The design intent of this building area is to create landmark state facilities that also relate to development to the north and east. Built forms should define the edge of the *Circle* as one of the Campus' major civic open spaces. A landscaped campus presence along Airdustrial Way S.W. is also sought.

USE State facilities

PLACEMENT Buildings in Area 2 are located around the southern edge of the *Circle*. The envisioned state buildings facing *Main Street* and the *Circle* should reinforce the Campus' urban character. Primary pedestrian entrances to the buildings shall be from the *Circle* and *Main Street*. Building setbacks shall be no more than fifteen feet from the right of way. Spaces between buildings should be made such that they support a variety of public uses, such as park or garden space, plazas or courtyards. Buildings that are set back from the *Circle* should provide hard-surfaced open space contiguous with and of the same materials (i.e. brick, stone) as the adjacent walk.

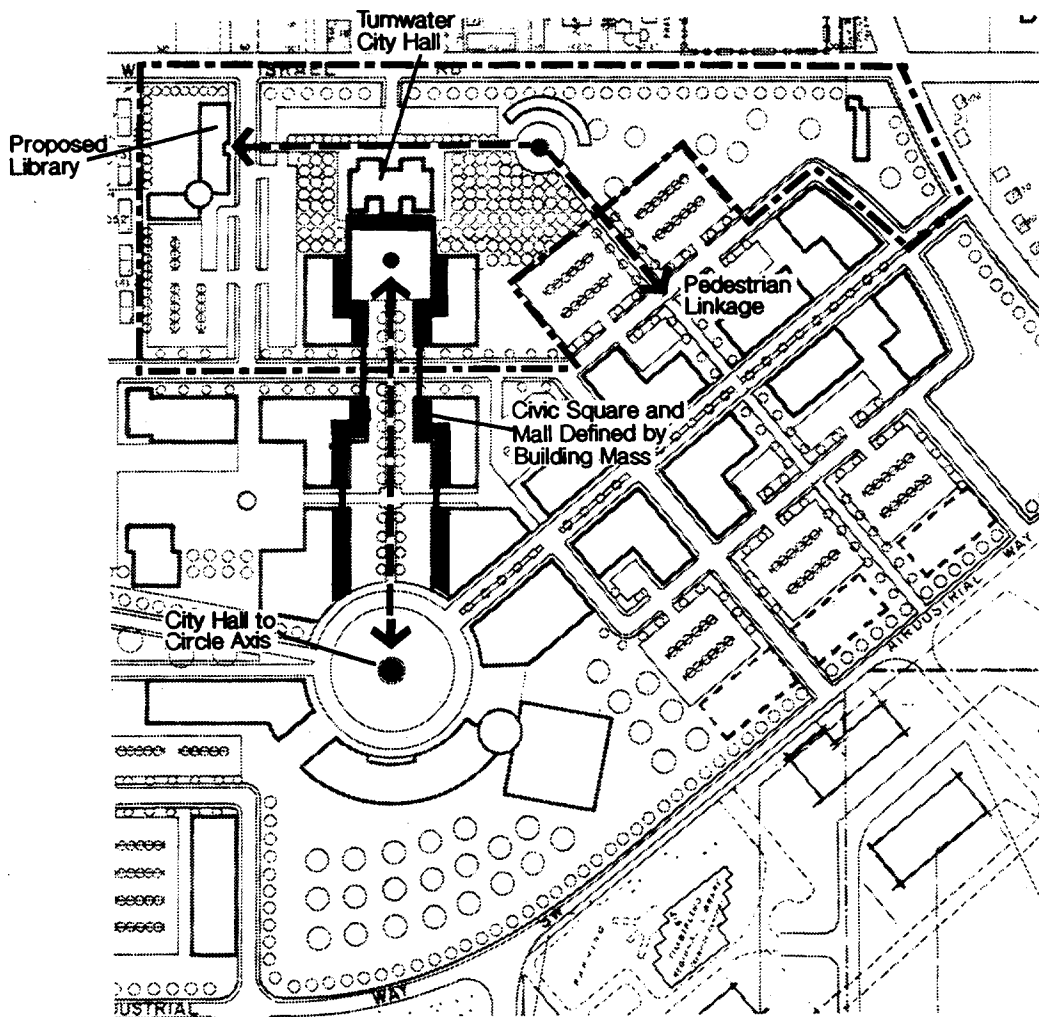
HEIGHT Buildings shall be a minimum of 45 feet and a maximum of 85 feet.

AREA 2**CHARACTER**

The state buildings of the Tumwater Campus will support, through their massing and spatial proportions, an urban organization of buildings and open spaces. Those state buildings that are adjacent to *Main Street* and the *Circle* should define the *Circle* as the Campus' civic center. These buildings should also form smaller open spaces that will each take on their own character. The building facade of the ground floor within view of *Main Street*, the *Circle*, or *Triangle* shall include windows and overhead protection. The building facade which orients north in line with City Hall should acknowledge that civic building. The use of blank walls of concrete cinder block and without facade ornamentation is not permitted. Building materials such as brick, masonry, glass, tile, stone, or wood is required on the building facade. Uninterrupted glass curtain walls and all-mirrored glass are not permitted.

PARKING

Parking in Area 2 shall be primarily accommodated by a structured parking garage. If this structure is located within thirty feet of either *Main Street* or the *Circle* edge, it shall provide ground level retail frontages. If the structure fronts the *Circle* or *Main Street*, setbacks shall be no more than 15 feet from the right-of-way in order to define the spaces. Otherwise, the structure will be effectively buffered from view from *Main Street* and the *Circle* with landscaping. Design of small areas of parking, separated by landscaped visual barriers, is acceptable. Design of large, unbroken expanses of parking shall be avoided. A minimum of 10 percent of parking areas shall be devoted to plantings and landscaping, which shall not be limited to the perimeter of the parking areas. No access to parking areas shall be directly from the *Circle* or *Main Street*.



AREA 3**INTENT**

Civic buildings on the Tumwater Campus represent one of the primary elements of its spatial organization. Their role in the physical and administrative functioning of the Campus is fundamental. The civic buildings of the Campus will support a structured urban organization of buildings and open space. Their character should reflect permanence and public accessibility and establish distinguishable landmarks.

USE

Administration of the City of Tumwater and other public community service uses.

PLACEMENT

Civic buildings shall define the open space corridor, or *Civic Mall*, that begins at the *Circle* and ends at the Tumwater City Hall. These civic buildings should create, through their placement, an urban *Civic Square* in front of City Hall and provide a pedestrian focal point. Their facades should emphasize the importance of the *Civic Square* and the *Civic Mall*. The library, as indicated on the plan, shall relate to the Pedestrian Way fronting City Hall which terminates at the library site. The Area 3 graphic identifies several alternatives for the location of the library, which could relate more significantly to the civic center of Tumwater. City buildings' ground floor entrances shall be oriented to the *Civic Square*. Spaces between buildings should be created to support a variety of public uses, such as parks or garden space, plazas or courtyards. Service functions should be oriented away from the *Civic Mall* and the *Civic Square*.

HEIGHT

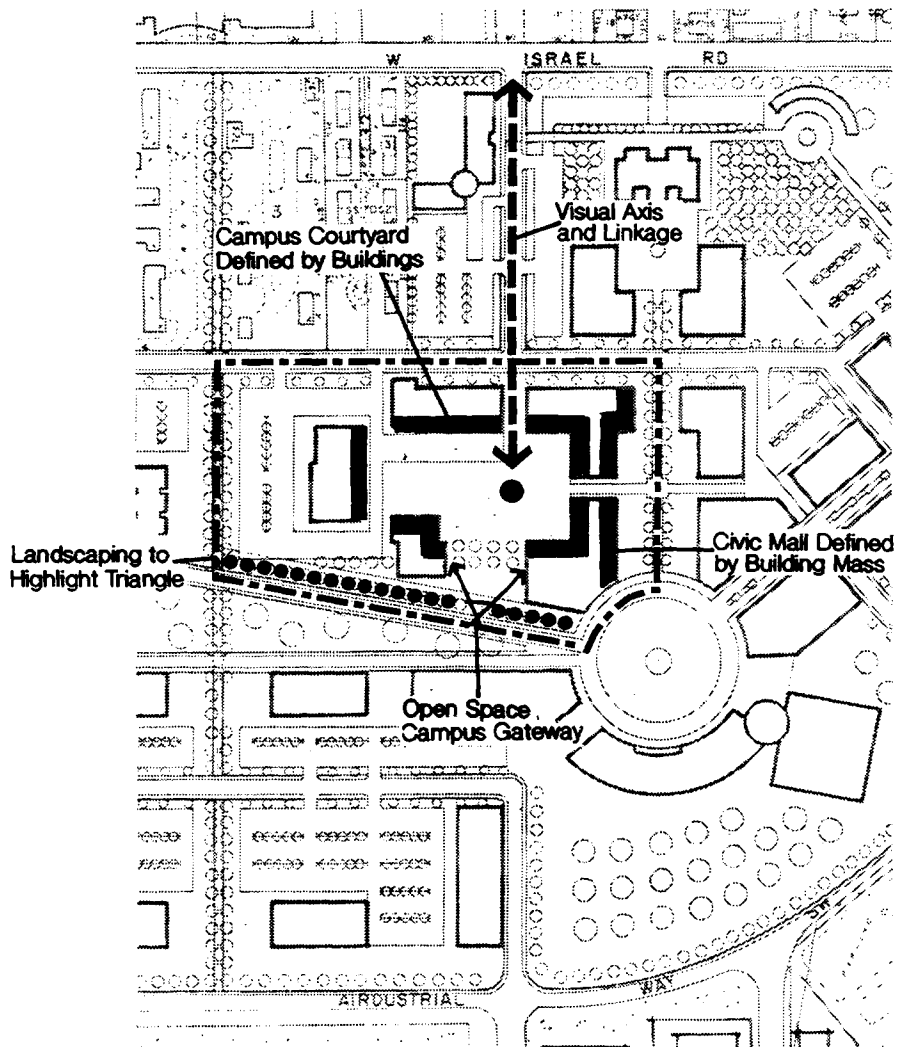
The Tumwater Campus civic buildings shall be a minimum of 30 feet and a maximum of 65 feet in height. Exceptions may be allowed for landmark features such as clock towers or cupolas that relate to the defined view axis.

AREA 3**CHARACTER**

The building facade of the ground floor facing the *Civic Square* shall include windows and overhead protection. The use of blank walls (such as concrete cinder block) without facade ornamentation is not permitted. Building materials such as brick, masonry, glass, tile, stone, or wood is required on the ground floor of building facades. Uninterrupted glass curtain walls and all-mirrored glass are not permitted.

PARKING

Parking shall be effectively buffered from view from the *Triangle* and the *Civic Mall*. Existing parking for City Hall, located along Israel Road and New Market Street, shall be maintained. Additional parking for future civic buildings should be placed to the rear of those buildings, away from the civic square, without sacrificing convenience or safety. No driveways shall cross the Pedestrian Way. Design of small areas of parking, separated by landscaped visual barriers, is encouraged. If required, structured parking may be utilized if not readily visible from public open spaces. Design of large, unbroken expanses of parking shall be avoided. A minimum of 10 percent of all parking areas shall be devoted to plantings and landscaping, which shall not be limited to the perimeter of the parking areas.



AREA 4**INTENT**

The design of the buildings of Area 4 should support an educational campus within the Turnwater Campus. A distinguish character should pervade these buildings. Their character should reflect permanence and establish recognizable landmarks.

USE

Educational and civic use

PLACEMENT

In Area 4, existing and future buildings shall define a Campus courtyard, which would provide an outdoor pedestrian focal point for the New Market Skills Center. The relocation of the Skills Center auto facility from the proposed *Triangle* right-of-way could initiate the courtyard formation, as shown in the Area 4 graphic. Future buildings would locate east of the existing and relocated Skills Center facilities. These buildings shall complete the courtyard and define the *Civic Mall* and *Circle*. Primary entries to these buildings shall be located on facades facing the *Civic Mall* and/or *Circle* open spaces.

HEIGHT

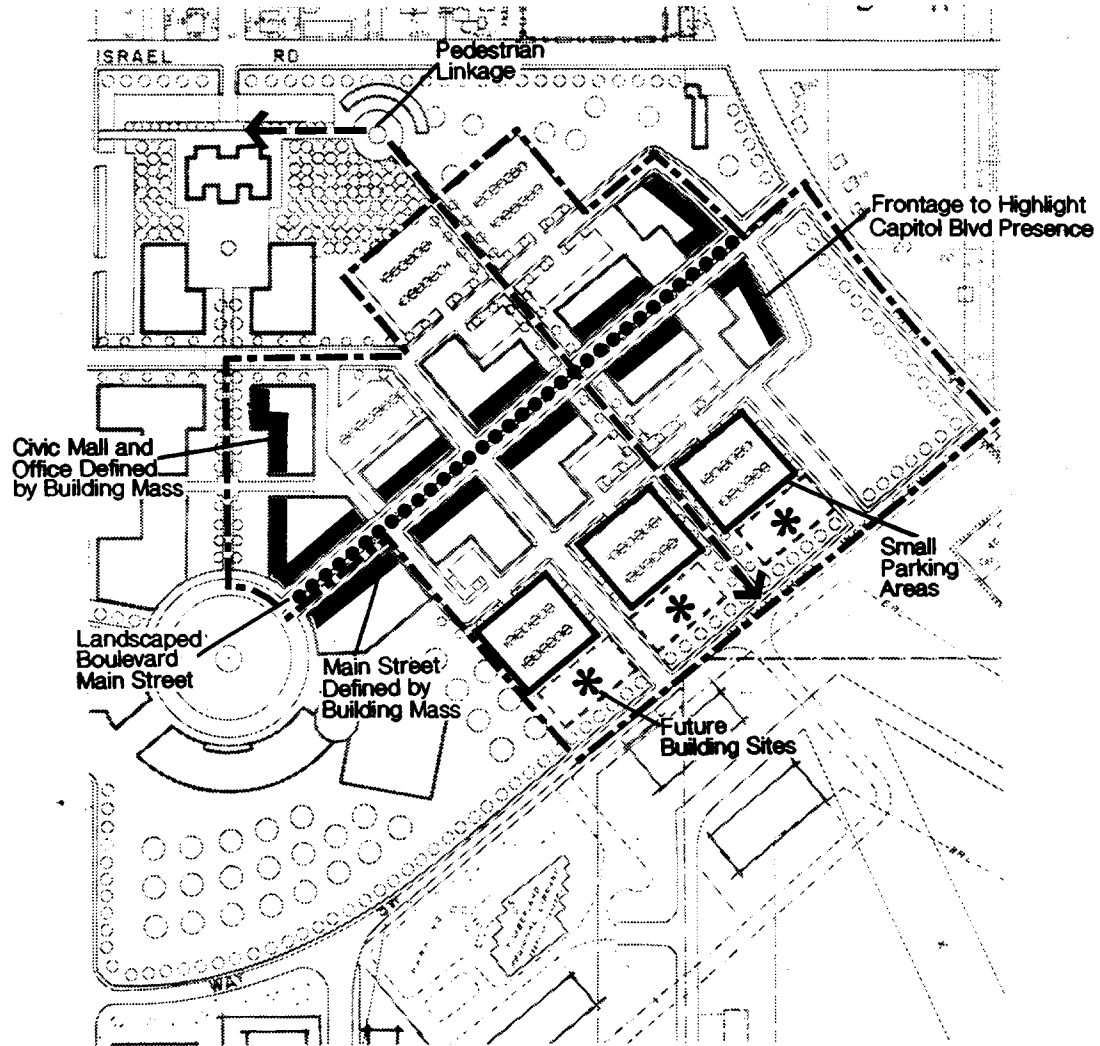
Buildings fronting the *Civic Mall* shall be a minimum of 40 feet and a maximum of 65 feet in height. Other Skills Center buildings shall be no less than 25 feet and no more than 50 feet in height. Taller landmark features may be permitted.

AREA 4**CHARACTER**

Facades of Skills Center and other civic buildings facing the *Civic Mall* should emphasize the importance of that axis as a significant civic open space. All of the Skills Center buildings of the Campus should support the academic courtyard. The building facade of the ground floor facing the *Civic Mall* and the *Triangle* shall include windows and overhead protection. The use of blank walls (such as concrete cinder block) without facade ornamentation is not permitted. Building materials such as brick, masonry, glass, tile, stone, or wood is required on the first floor of all building facades. Uninterrupted glass curtain walls and all-mirrored glass are not permitted.

PARKING

Parking for the Skills Center shall be primarily located along the pedestrian path, and away from the *Civic Mall* and *Circle*. A landscaped screening buffer of at least five feet in height shall be located along the entire length of the parking area adjacent to the pedestrian path. A new access road will link the right-of-way to the Skills Center Auto Facility, as shown in the Area 4 graphic. In general, design of small areas of parking, separated by landscaped visual barriers, is encouraged. Structured parking may be utilized if not readily visible from public open spaces. Design of large, unbroken expanses of parking shall be avoided. A minimum of 10 percent of parking areas shall be devoted to plantings and landscaping, which shall not be limited to the perimeter of the parking areas. Parking structures shall be effectively buffered from view from the *Triangle*.



AREA 5

- INTENT** In Area 5, a tree-lined boulevard, defined by retail/commercial structures, will establish a *Main Street* traditional town center character. The district should further reinforce a downtown main street character through its vegetation, lighting, furniture, physical proportions, and services provided.
- USE** Commercial/retail use
- PLACEMENT** Area 5 buildings facing the *Circle* or *Main Street* should reinforce an urban downtown character. Their primary entrances shall be from *Main Street* or the *Circle*. The building facade on the ground floor on *Main Street* shall include windows and overhead protection. Setbacks of those buildings shall be no more than five feet from the right of way. Spaces between buildings should be created to support a variety of public uses, such as parks or garden space, plazas or courtyards. Future buildings located along Airdustrial Way S.W. must provide facades to that road that reinforce the character of Area 5 and provide pedestrian frontage and access equal to the primary facades of other Area 5 buildings. Buildings fronting *Main Street* shall all be developed before any additional buildings are located along Airdustrial Way S.W..
- HEIGHT** Buildings along *Main Street* or on the *Circle* shall be a maximum of 45 feet. Buildings along Airdustrial Way S.W. shall be a minimum of 30 feet and a maximum of 60 feet. Buildings along the *Civic Mall* shall be a minimum of 40 feet and a maximum of 65 feet. The width of the *Main Street* should be proportionate to the adjacent buildings and satisfy functional traffic needs.

AREA 5**CHARACTER**

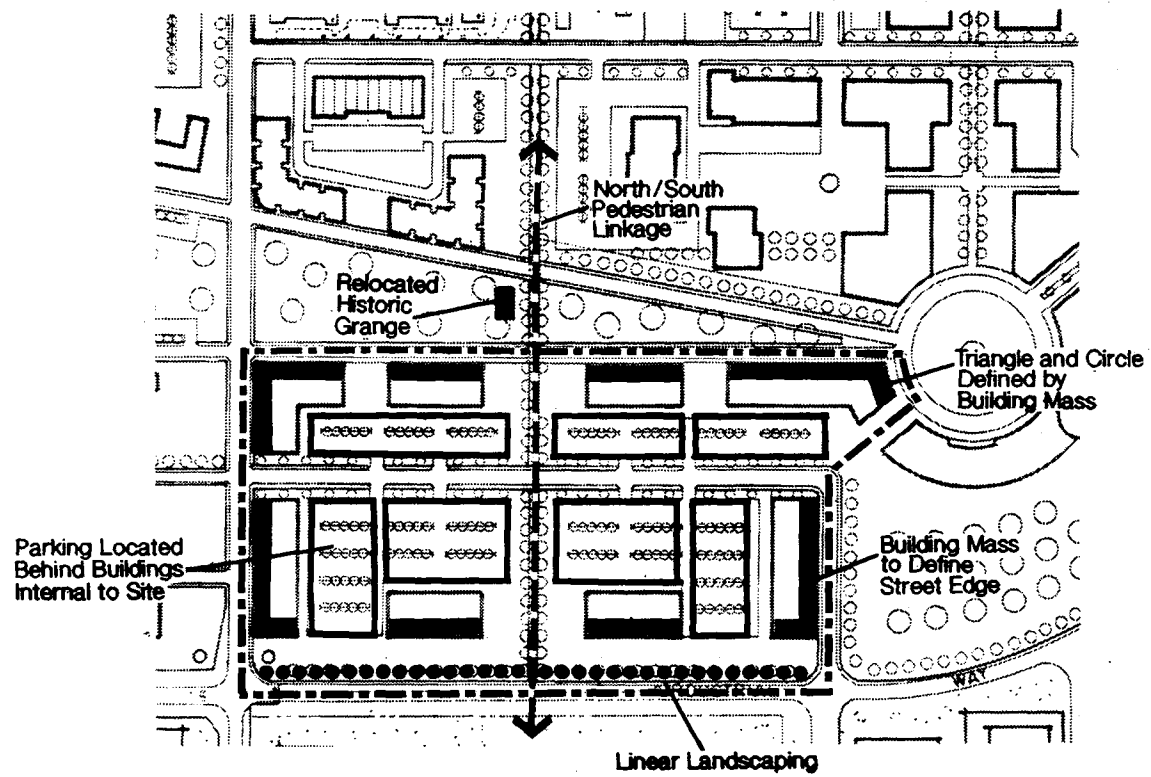
In Area 5, commercial buildings will define a new but traditional *Main Street* town center. The buildings should define the urban quality of the *Boulevard*, promoting a vehicular and pedestrian linkage between *Capitol Boulevard* and the *Circle*. Along the *Civic Mall* and the *Circle*, buildings should define these open spaces. All buildings facing *Main Street*, the *Circle*, and the *Civic Mall* shall include windows and overhead protection. Where appropriate, buildings should also form smaller open spaces which will each take on their own character. The use of blank walls [such as concrete cinder block] without facade ornamentation is not permitted. Building materials such as brick, masonry, glass, tile, stone, or wood is required on the building facade. Uninterrupted glass curtain walls and all-mirrored glass are not permitted.



VIEW FROM "MAIN STREET" LOOKING WEST TO CIRCLE AND STATE BUILDING

AREA 5**PARKING**

All parking in this commercial area shall be located behind buildings. No driveways to parking areas shall be directly from the *Circle* or the *Boulevard*. Design of small areas of parking, separated by landscaped visual barriers, is encouraged. Otherwise, structured parking may be utilized, if not readily visible from public open spaces. Design of large, unbroken expanses of parking shall be avoided. A minimum of 10 percent of parking areas shall be devoted to plantings and landscaping, which shall not be limited to the perimeter of the parking areas. Parking structures shall be effectively buffered from view from the *Circle* and the *Boulevard* or have ground-level retail commercial services facing the pedestrian street. They should follow the restrictions governing other Area 5 buildings facing *Main Street*.



AREA 6

INTENT In Area 6, buildings should provide commercial and retail services, and potentially state facilities, that complement the Campus. These buildings should reinforce the importance of the *Triangle* and the *Circle* as significant community open spaces.

USE Mixed-use, commercial and retail, and, potentially, state facilities

PLACEMENT Buildings are located at the perimeter with parking at the center of the subarea. Buildings along the *Triangle* shall have their primary entrances facing this open space. These buildings shall be located no more than five feet from the right-of-way. Buildings fronting Airdustrial Way S.W. shall be set back a minimum of 20 feet and a maximum of 30 feet to allow for a green buffer between Airdustrial Way S.W. and the Campus. Spaces between buildings should be made such that they support a variety of public uses, such as park or garden space, plazas or courtyards.

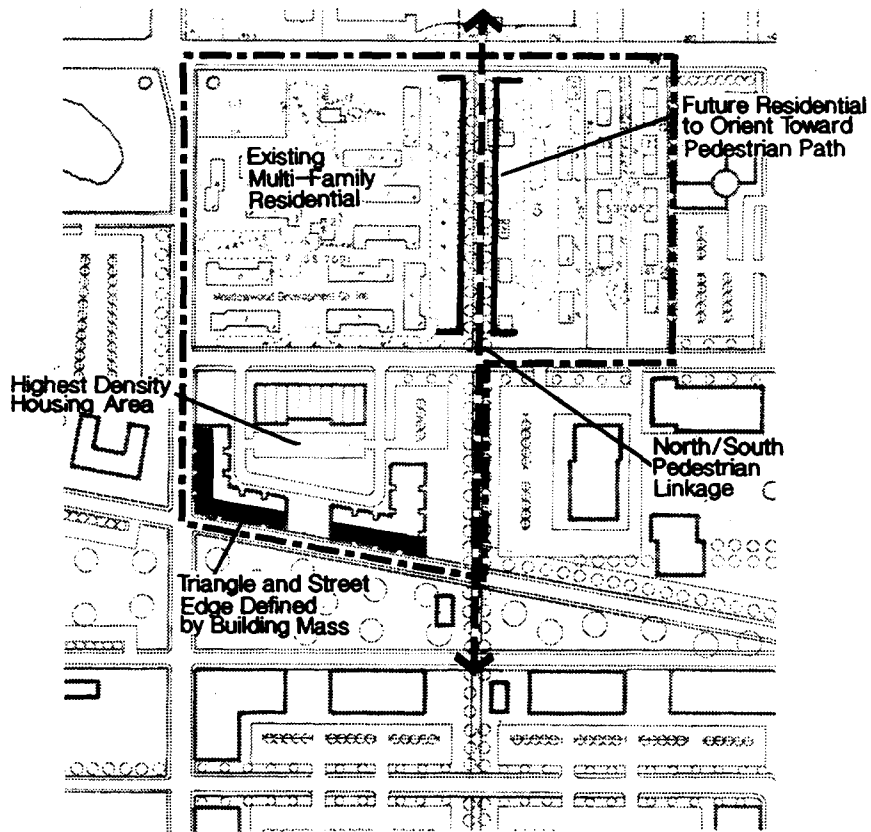
HEIGHT For Area 6, buildings along the *Triangle* or on the *Circle* shall be a minimum of 40 feet and a maximum of 65 feet. Buildings along Airdustrial Way S.W. shall be a minimum of 30 feet and a maximum of 65 feet.

AREA 6**CHARACTER**

The facades of those buildings on the *Circle* and on the *Triangle* should emphasize the importance of both the *Circle* as a terminus to the *Triangle* and the urban quality of the Campus itself. These buildings should also form smaller open spaces that will each take on their own character. Buildings located along Airdustrial Way S.W. must provide facades to the road that reinforce the character of the Campus. The development should not have the character of an auto-oriented, commercial strip mall. The use of blank walls (such as concrete cinder block) without facade ornamentation is not permitted. Building materials such as brick, masonry, glass, tile, stone, or wood are required on the ground level of buildings. Uninterrupted glass curtain walls and all-mirrored glass are not permitted.

PARKING

Design of small areas of parking, separated by landscaped visual barriers, is encouraged. Otherwise, structured parking may be utilized, if not readily visible from public open spaces. Design of large, unbroken expanses of parking shall be avoided. A minimum of 10 percent of parking areas shall be devoted to plantings and landscaping, which shall not be limited to the perimeter of the parking areas. Parking structures shall be effectively buffered from view from the *Triangle* and the *Circle* or have ground-level retail commercial services facing the pedestrian street and follow the restrictions governing the other Area 6 buildings facing the *Triangle* and *Circle*. No access to parking areas shall be directly from the *Circle* or the *Triangle*. A new east/west access road is provided. The north/south pedestrian corridor must be established through the parking area.



AREA 7**INTENT**

Existing and future residential buildings on the Tumwater Campus should provide a mix of scales and housing types: single-family detached units, multi-family units, and townhouses. This mix is intended to enable families of surrounding areas, state employees, and people affiliated with the Campus a wide choice of housing opportunities. These residences are intended to promote 24-hour-a-day activity on the Campus. A vertical mix of uses may include upper-level residential over retail and commercial development. This integration of residents into the commercial areas would strengthen the twenty-four-hour presence of residents in the Campus community. Generally, density increases from single-family detached units north of Israel Road to multi-family structures facing the *Triangle*.

USE

Neighborhood retail services on the first floor are encouraged in multi-family frontage on the *Triangle* and Linderson Way. Single-family and townhouse development may not locate any commercial or retail services in any part of their buildings.

PLACEMENT

Facades on the *Triangle* and Linderson Way shall be built to those property lines and those elevations shall occupy no less than 80 percent of the frontage. Townhouses can be set back a maximum of twenty feet from the street and access should be from street-facing elevations. The multi-family and townhouse residences shall collectively define an open space, as shown in the Area 7 graphic. Existing residences occupy the north-central part of the Campus, east of the Labor & Industries Building, and north of the New Market Vocational Skills Center. As the form of the Campus is realized, there may be dislocation of existing housing. Newly constructed housing should orient to the *Pedestrian Way*. Housing facing the *Pedestrian Way* must include semi-private transitional space at all entries on the *Pedestrian Way*. These may include covered and uncovered porches, steps with railings, verandas, and garden paths. Setbacks from the *Pedestrian Way* shall be a minimum of five feet and a maximum of fifteen feet.

HEIGHT

Multi-family housing on the *Triangle* and Linderson Way shall be a minimum of 30 feet and a maximum of 60 feet in height. Townhouses and multi-family units shall be a minimum of 25 and a maximum of 45 feet in height. Single-family homes shall be a maximum of 35 feet tall.

AREA 7**CHARACTER**

Multi-family housing shall promote an urban character along the *Triangle* and Linderson Way. For these structures, glazing material is encouraged on ground-level facades facing the *Triangle*. The setback of any new townhouse between the building and the sidewalk should consist of an enclosed entry garden, such that all enclosures shall be of equal height.

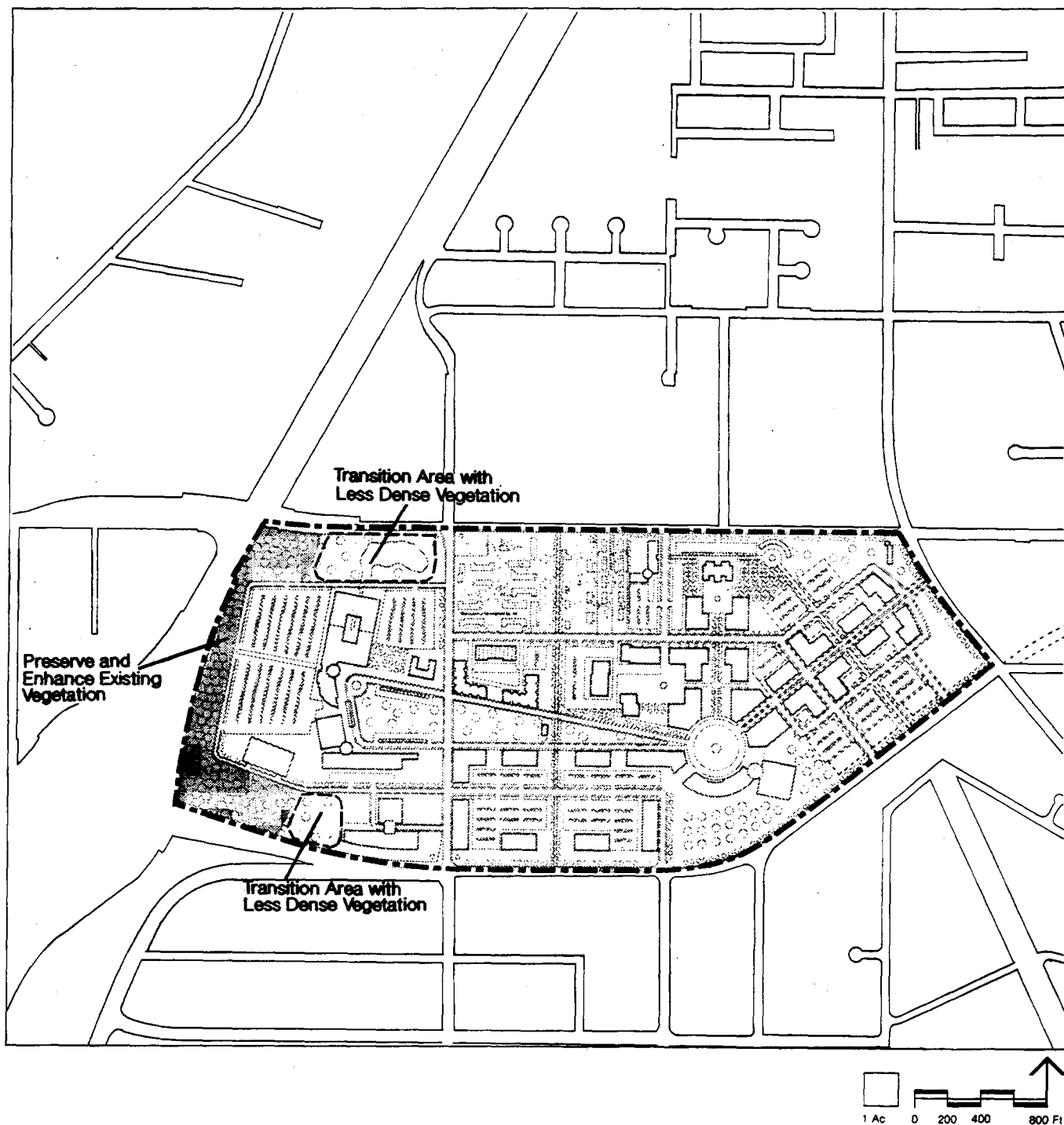
PARKING

Design of small areas of parking, separated by landscaped visual barriers, is encouraged. Otherwise, structured parking may be utilized, if not readily visible from public open spaces. Design of large, unbroken expanses of parking shall be avoided. A minimum of 10 percent of parking areas shall be devoted to plantings and landscaping, which shall not be limited to the perimeter of the parking areas. Parking areas and structures shall be effectively buffered from view from the *Triangle*. Any new construction of housing on the Tumwater Campus should provide driveways incorporating joint access with adjacent properties, if possible. These drives shall provide access to garages or parking areas within or at the rear of those buildings.

The design of the landscape of the Tumwater Campus is critical to the realization of a quality mixed use development. The diversity of activities planned for the Campus should each present their own character. Building facades on the Campus will greatly determine the image the Campus will take. The spacing of buildings and the resultant areas between them are equally important, as are the larger open spaces. The ornamental and space-forming qualities of water features, sculpture, landforms, smaller structures, and vegetation will ultimately establish the character and the quality of the Campus. Design guidelines address the following landscape topics:

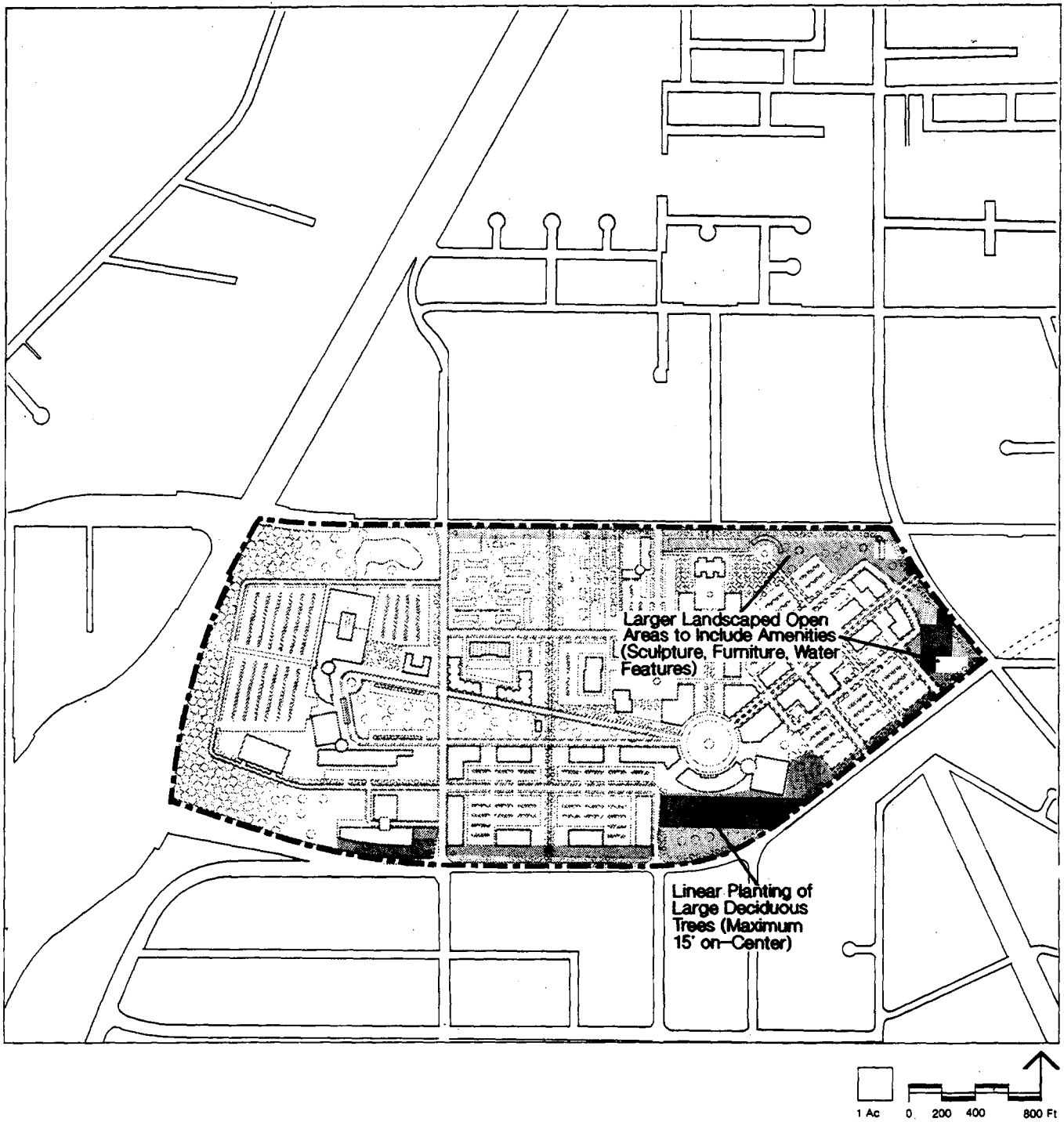
- Buffer zone
- Perimeter zone
- Gateways
- Park blocks
- Pedestrian ways
- Circle
- Triangle

The intent of each element, its location, and design criteria are described.



BUFFER ZONE

- INTENT** The purpose of the *Buffer Zone* is to separate and protect development from adjoining high-traffic corridors, to preserve and enhance vegetation, and to create an image and presence of the Campus along its freeway frontage.
- LOCATION** The *Buffer Zone* is located in the western portion of the Campus and generally separates the planned state facility development from the freeway, Airdustrial Way S.W., and Israel Road.
- CRITERIA** Existing vegetation, including large trees and ground covers, shrubs, and small trees, should be maintained in a natural state. Additional vegetation should supplant existing where buildings or parking lots diminish the breadth or density of the *Buffer Zone*. Roads and paths through the *Buffer Zone* are discouraged. In this regard, all new building siting should recognize the importance of vegetation preservation. The combination of dense vegetation, berms, or screen walls should attenuate noise impacts.



PERIMETER ZONE**INTENT**

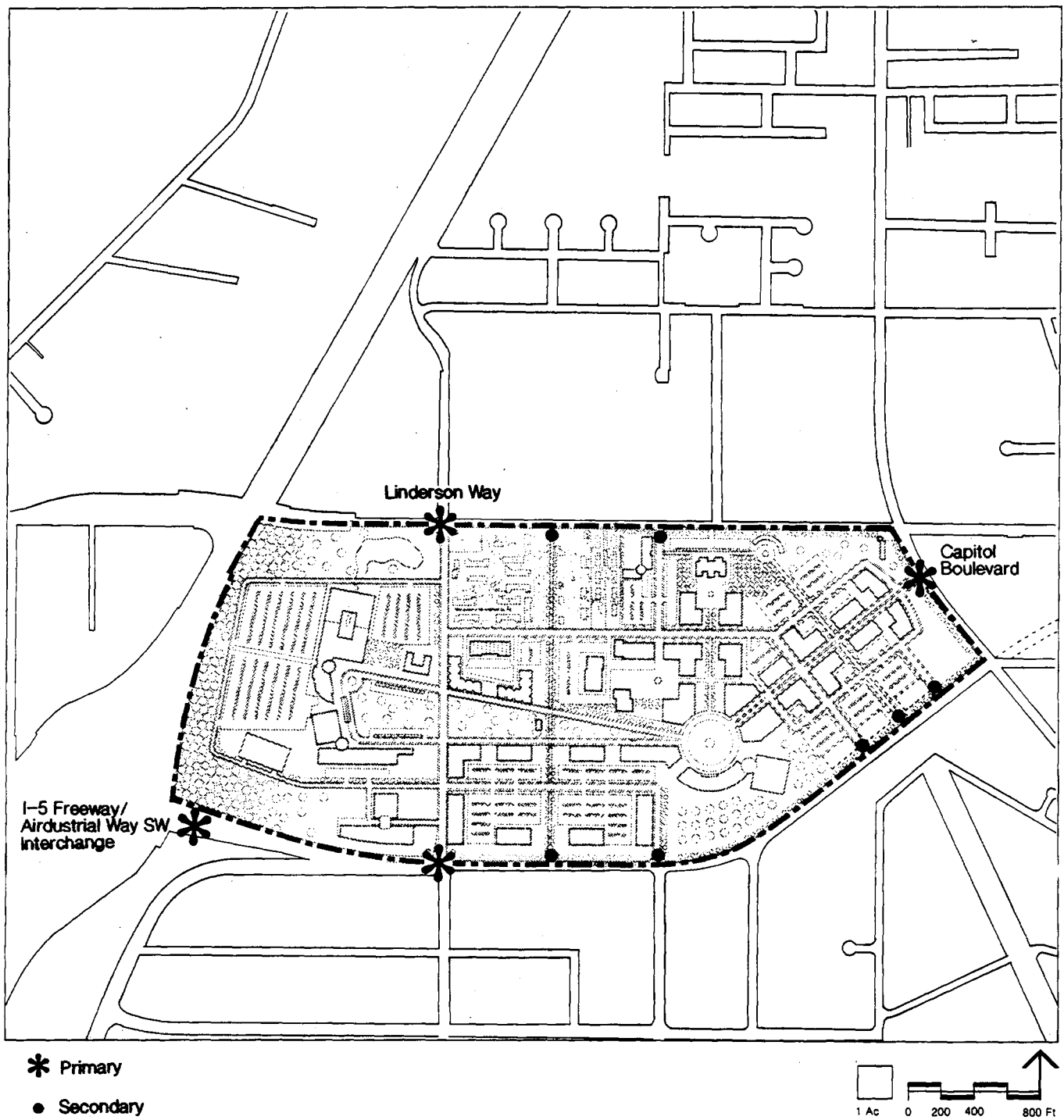
Landforms, water features, vegetation, and small structures at the edges of the Tumwater Campus should enhance the urban civic image of the Campus. Plantings around the perimeter should help to enclose and define the Campus edges. Views into the Campus from the surrounding area should not be screened. The edge transition should be highlighted but is not intended as a barrier. Relationships with the surrounding areas should be created.

LOCATION

The *Perimeter Zone* is defined as the Campus edges along Israel Road, Capitol Boulevard, and Airdustrial Way S.W. The depth of the *Perimeter Zone* will vary, depending upon the specific location. The area addressed is within the Campus boundaries, but it is recommended that improvements be coordinated with the opposite sides of the perimeter streets.

CRITERIA

Perimeter plantings should be of large deciduous trees that will, when mature, frame views into the Campus between their trunks and beneath their canopy. Due to the close proximity of Israel Road, Capitol Boulevard, and Airdustrial Way S.W., the perimeter vegetation must be pollution-resistant and salt-tolerant. It should be set back from the road edge a maximum of six feet and should be spaced a maximum of 20 feet on-center. In the interest of maintaining an urban character in the Campus, low ground covers and flowers may be used along the perimeter. Shrubs should be used only where they will not impede sight lines into the Campus or cause safety hazards at street intersections. Mounds and berms along the perimeter may not exceed 30 inches in height. Perimeter structures may be used to generate a rhythm or accentuate the edge of the Campus, but they should not unduly inhibit views into or out of the Campus. Any surface parking areas should be effectively screened with buffer vegetation.



GATEWAYS**INTENT**

Gateways to the Campus should accentuate and define linkages between the Campus and the surrounding communities. They should establish a hierarchy along the perimeter edge by punctuating that edge with special structures, plantings, earthworks, and water features.

LOCATION

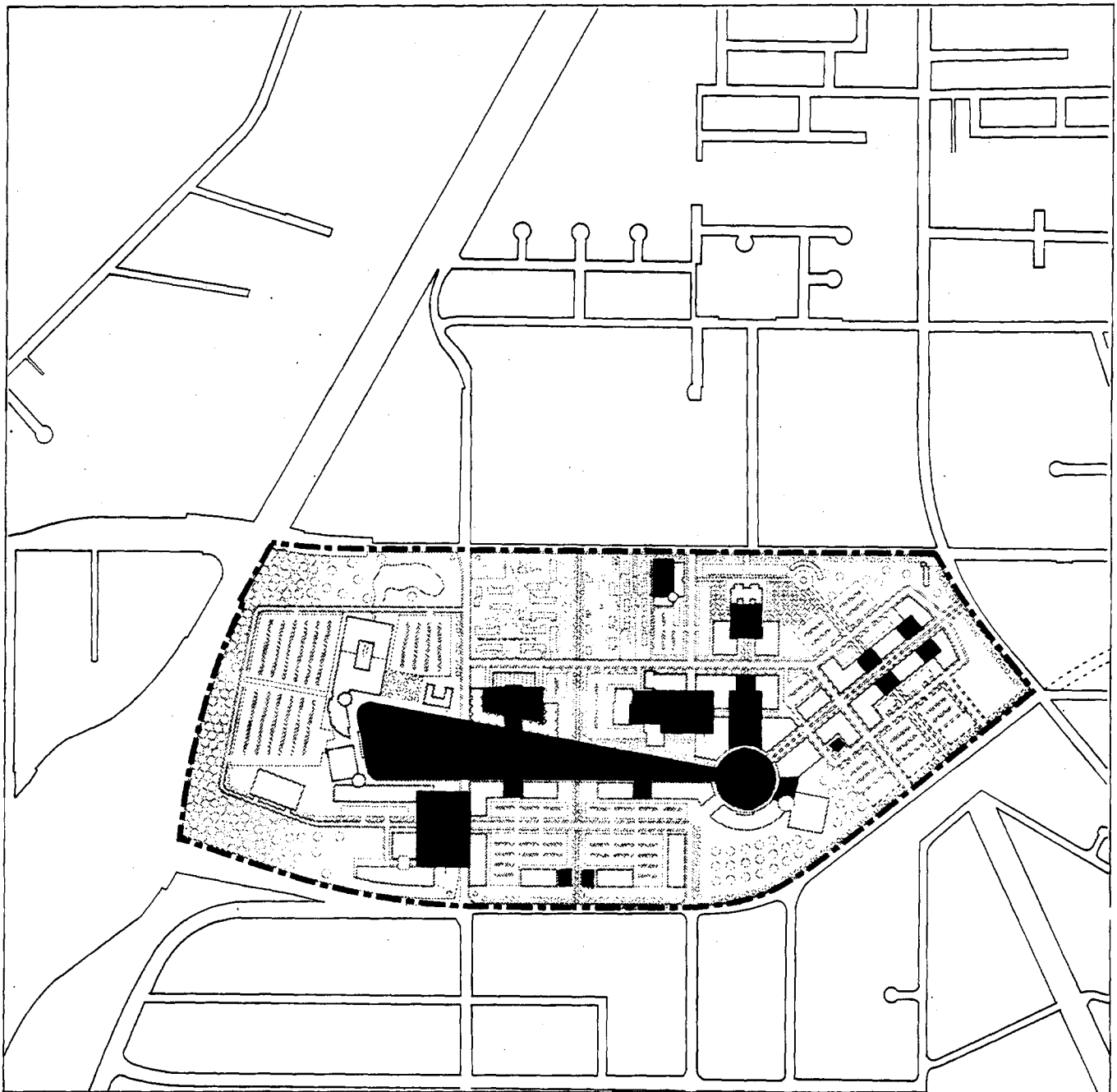
Primary Gateways into the Campus occur at the intersections of Linderson Way and Israel Road, Capitol Boulevard, and the Campus Boulevard, and at Linderson Way and Airdustrial Way S.W..

Secondary Gateways into the Campus occur at the two *Pedestrian Ways* on Airdustrial Way S.W., the two *Pedestrian Ways* on Israel Road, the vehicular entry to Tumwater City Hall off Israel Road, and the three connector road entries off Airdustrial Way S.W..

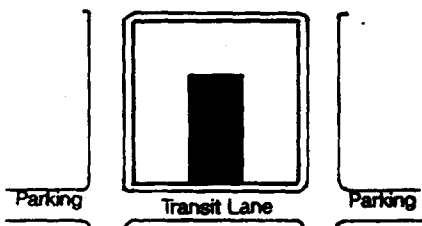
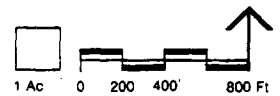
CRITERIA

The *Gateways* should be made of solid elements, should be visible from every angle of approach, and should allow a change of light, surface, and view while passing through. *Primary Gateways* should reflect the importance of those points at which most pedestrian and vehicular traffic enters and leaves the Campus. They should reflect the image of the Campus community as a whole. *Secondary Gateways* should reflect the more specific areas at which they occur, through placement, proportions, and materials.

Recommended *Gateway* elements include arches, pergolas, trellises, canopies, gatehouses, wrought or cast iron fencing with stone or brick piers, fountains, pools, special paving and groundcovers, changes in elevation of roadways and surrounding areas, sculpture, vegetative bosques, copses, allees, and border gardens.

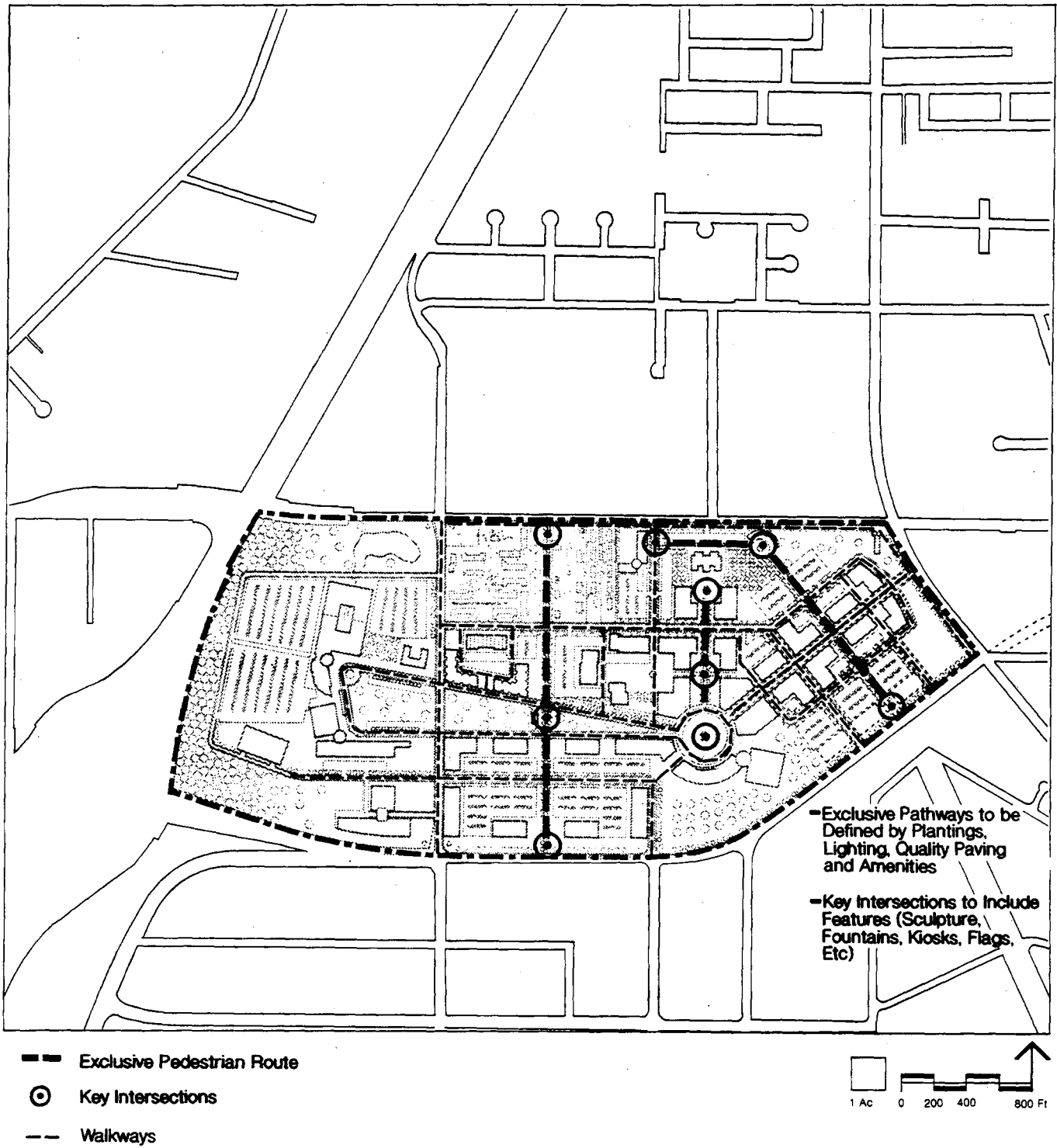


'Park Block' is defined as a grouping of buildings which defines a central open space commons which opens to a pedestrian and vehicular artery.



PARK BLOCKS

INTENT	Consistent with the design concept described in the State Capitol Master Plan, <i>Park Blocks</i> are intended to provide landscaped open spaces that are defined by their adjoining buildings. The <i>Park Blocks</i> are intended to promote public transit as a primary transportation mode.
LOCATION	<i>Park Blocks</i> are located throughout the Campus. Included is the space surrounded by the Newmarket Vocational Skills Center, the plaza before City Hall, the <i>Civic Mall</i> between City Hall and the <i>Circle</i> , the space between Phase 1 and Phase 2 of Labor & Industries, and pockets formed by buildings, vegetation, and landforms across the Campus. Locations cannot be precisely defined because the plan is conceptual and individual building sites have not yet been designed. Nonetheless, <i>Park Blocks</i> must be located and included as part of specific project development. They may vary in size and have different attributes but must meet the basic intent.
CRITERIA	<i>Park Blocks</i> are landscaped outdoor rooms enclosed by buildings and vegetation which may range in size from a very small, intimate courtyard to a several-acre quadrangle. They may have surfaces of lawn with paths and pools, or paved entirely with brick or granite. Each space should have its own character. They will raise the importance of outdoor areas from leftover space to spaces as significant, positive, and necessary as those occupied by buildings. Due to the mix of types of open spaces, the <i>Park Blocks</i> will provide people working in the Campus, residents, and others visiting the Campus, varied outdoor opportunities. The <i>Park Blocks</i> will include places of quiet to sit and talk away from a busy office or shopping street; more open areas to relax and watch people go by; larger spaces to stroll or exercise. <i>Park Blocks</i> should not be adjacent to parking lots without significant visual and noise barriers, nor should they be near large vehicular routes. They should be located on or near pedestrian walkways, including the <i>Triangle</i> , the <i>Circle</i> , and <i>Main Street</i> .



PEDESTRIAN WAYS

INTENT *Pedestrian Ways* are intended to provide direct, safe, landscaped routes, free of barriers and automobiles, for pedestrians and bicyclists across the Campus. Because the pace is slower on a *Pedestrian Way*, the design of the way will be more densely detailed for all physical elements, including paving, seating and other furniture, lighting, plantings, signage, enclosures such as fencing and walls, water features, and landforms.

LOCATION *Exclusive Pedestrian Ways* extend through the site at three key locations: from Israel Road at the residential edge directly through the center of the Campus to Airdustrial Way S.W.; from between Israel Road and Tumwater City Hall parallel to Israel Road, to a small hub, then directly through the commercial/retail area, parallel to Capitol Boulevard, on to Airdustrial Way S.W.; from City Hall to the Circle. *Pedestrian Ways* (sidewalks) are also located throughout the Campus along the street system.

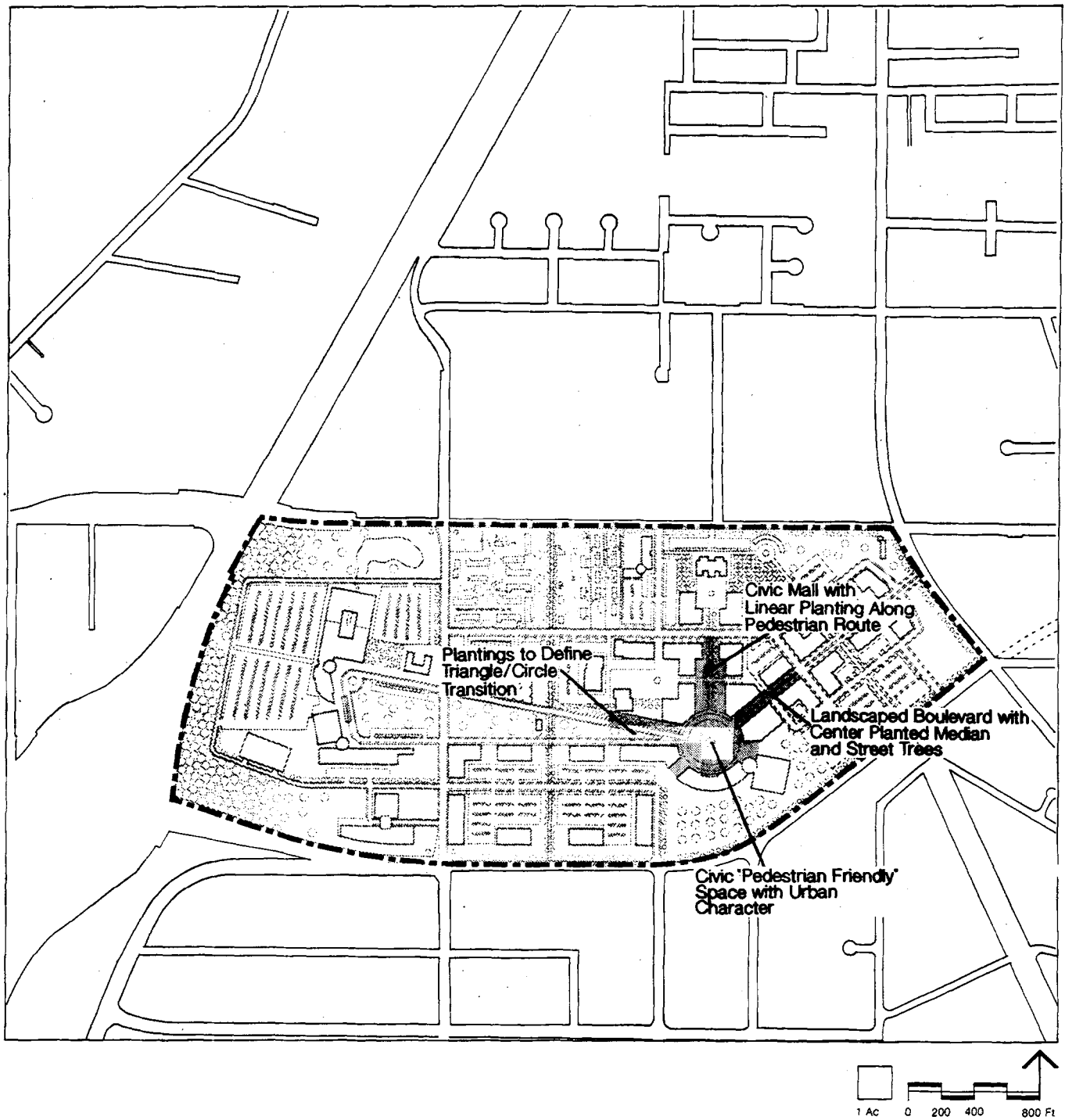
CRITERIA All exclusive pedestrian walkways should be at least 12 feet wide. *Pedestrian Ways* will have a right-of-way of at least 18 feet, to include three feet of planting area on each side of the paved walkway.

The setback of adjacent buildings will greatly determine the character of a pedestrian way. Setbacks along the *Pedestrian Ways* should be narrow, to help define and enclose the paths, unless the setback also includes a *Park Block* adjacent to the *Pedestrian Way*, in which case the setback may be greater.

The *Pedestrian Ways* all should be lined with deciduous trees along their length to help define and contain the paths and to provide shade. These trees should be placed no further apart than 15 feet on-center.

All paths should have effective lighting, and seating along their entire length.

Crosswalks should occur at all street crossings, and should be of the same material as the walkways. Paving should be distinctive and of durable materials. It should not be of bituminous rock, but rather brick, marble, granite pavers or cobbles, or individual brick-sized concrete pavers. At major intersections or the terminus of a path, a significant element, such as a sculpture or fountain, should identify that special portion of the path.



CIRCLE

INTENT

The *Circle* is intended to be the central hub of activity within the Tumwater Campus. From the *Circle*, the *Triangle* radiates to the west, the *Civic Mall* to the north, and the *Boulevard* to the east. In order to emphasize the importance of the *Circle*, and to work in concert with the unified building facades around the *Circle*, an integrated landscape treatment must also be applied.

LOCATION

The *Circle* is located at the center of the Campus, between the *Triangle* and the *Boulevard*.

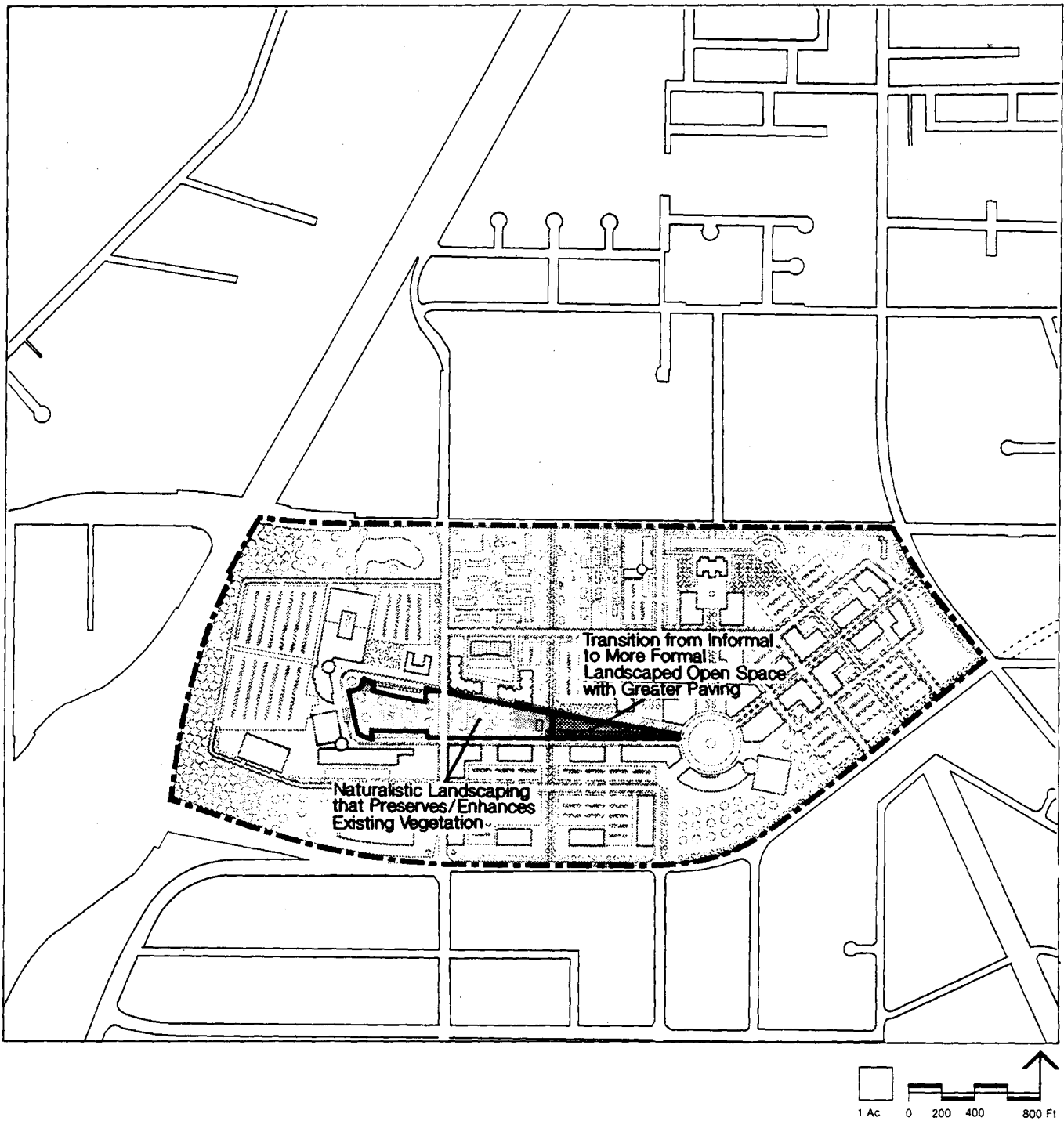
CRITERIA

Landscaping of the *Circle* should include large deciduous trees that follow the circumference of the outside of the *Circle* roadway, with a maximum setback from the curb of five feet and with maximum spacing of fifteen feet. These trees should be pollution-resistant and salt-tolerant.

Street furniture, including benches, lighting, trash receptacles, hydrants, tree grates should reflect the civic qualities of this area, and reference the history of Tumwater.

The area within the *Circle* should allow pedestrian use and enjoyment. It should contain elements of ornamentation such as fountains and/or sculptures that are scaled to reflect the size and importance of the *Circle* as a civic core. General views across the *Circle* should not be impeded.

No design for the area within the *Circle* is specified in detail. Rather, this area should be subject to more rigorous design analysis in the future, consistent with the described criteria.



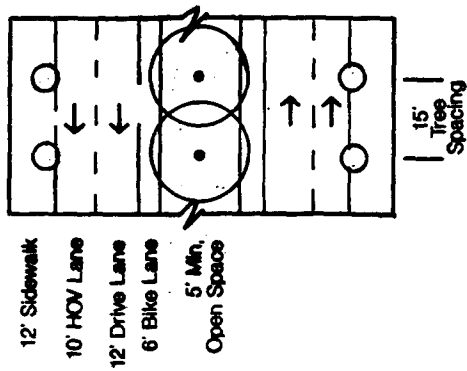
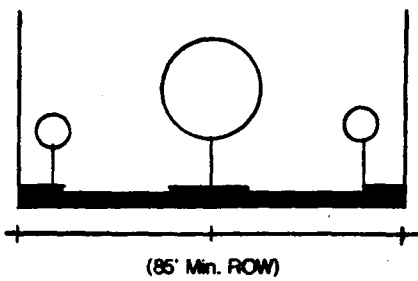
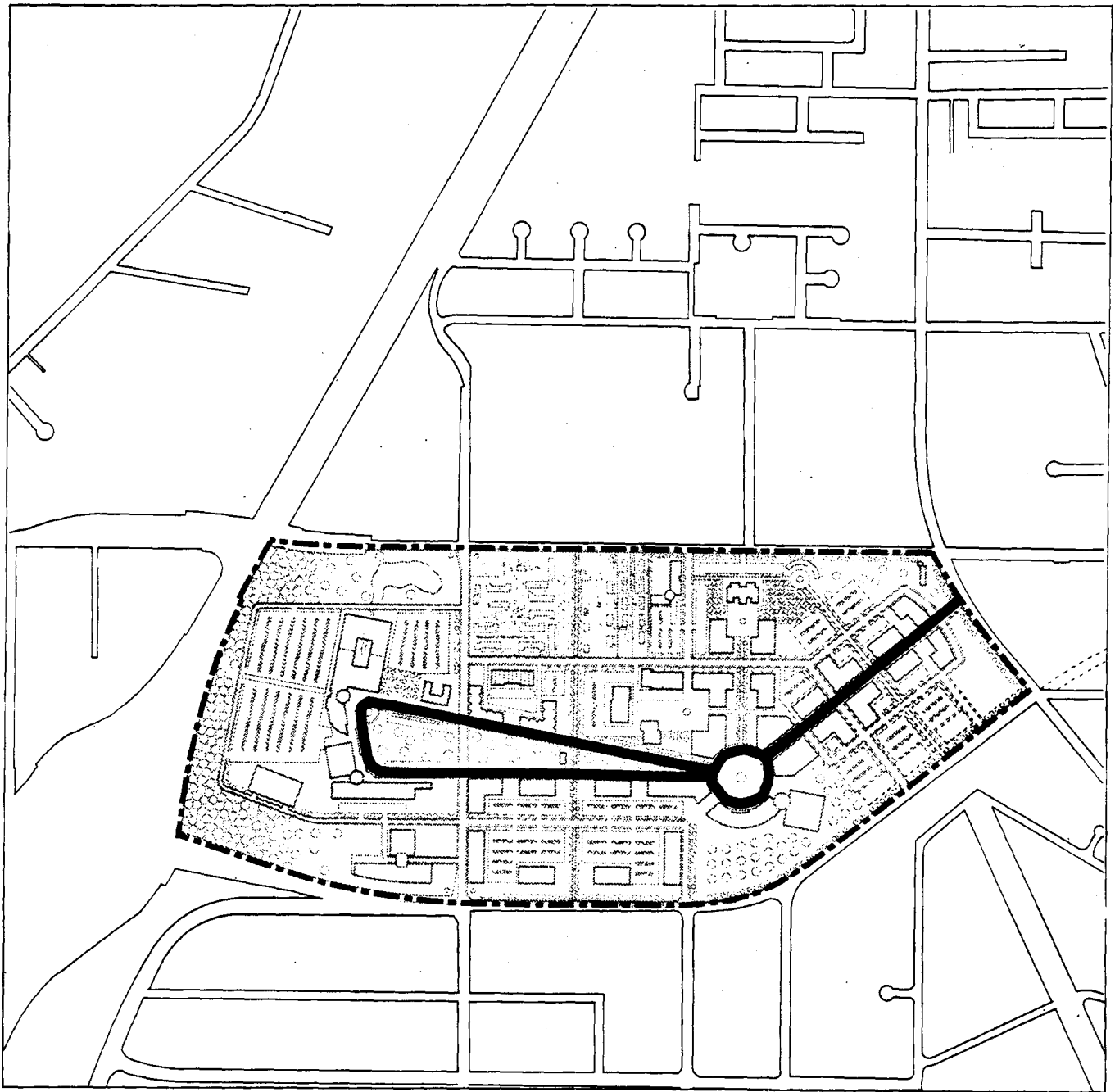
TRIANGLE

INTENT The *Triangle* is to be an open, bucolic park at the center of the Tumwater Campus. The easternmost third of the *Triangle* should act as the transition zone between the sylvan qualities of the western portions of the *Triangle* and the urban qualities of the *Circle*.

LOCATION The *Triangle* is located through the middle of the Campus, west of the *Circle*.

CRITERIA Vegetation on the western two-thirds of the *Triangle* should be maintained. Thinning should occur to allow sight lines through the *Triangle* and to create clear spaces on the *Triangle* for specific uses. Trees, especially large specimens, should be retained. Plantings of large trees on the entire edge of the *Triangle* should be installed to better define its open space character. The eastern third of the *Triangle* should have more paving, with brick, marble, granite, or another quality stone paving material. It should not be paved with concrete or bituminous. The same criteria apply for paving of paths throughout the entire Campus.

Because parking will be displaced with the creation of the *Triangle*, a maximum of 30 parking spaces will be provided within the *Triangle*. These spaces will be located along the perimeter of the *Triangle* in two areas: just south of the Child Development Center; and directly south of that, across the *Triangle*. These parking areas shall be screened from view from both within and outside the *Triangle*. The screen will be vegetative and will enhance the form of the *Triangle* along its edge through use of medium-to-large trees.



In addition to providing pedestrian and vehicular circulation, the system of Campus roads provides the framework for defining and uniting the various Campus areas. The "Plan Elements" section provided a general discussion of the circulation system. The following describes each road type in more detail. Additional engineering design and review of the streets may be necessary relative to standard street sections and adopted design standards. Changes may be approved as part of the design review process.

All roadways should have plantings, seating, trash receptacles, hydrants, tree grates, light standards, and bus stops, in scale with the proportions of the roadway and the area in which they are found, and designed to reflect the civic character of the Campus and the history of Tumwater.

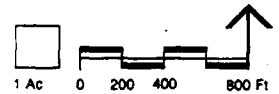
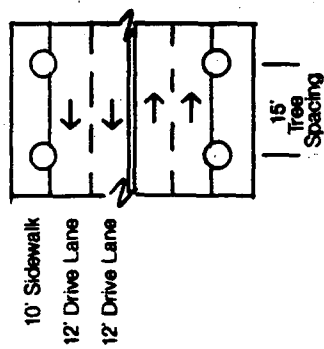
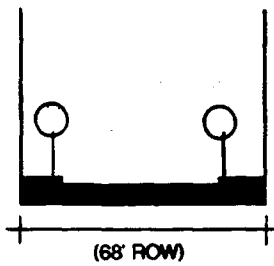
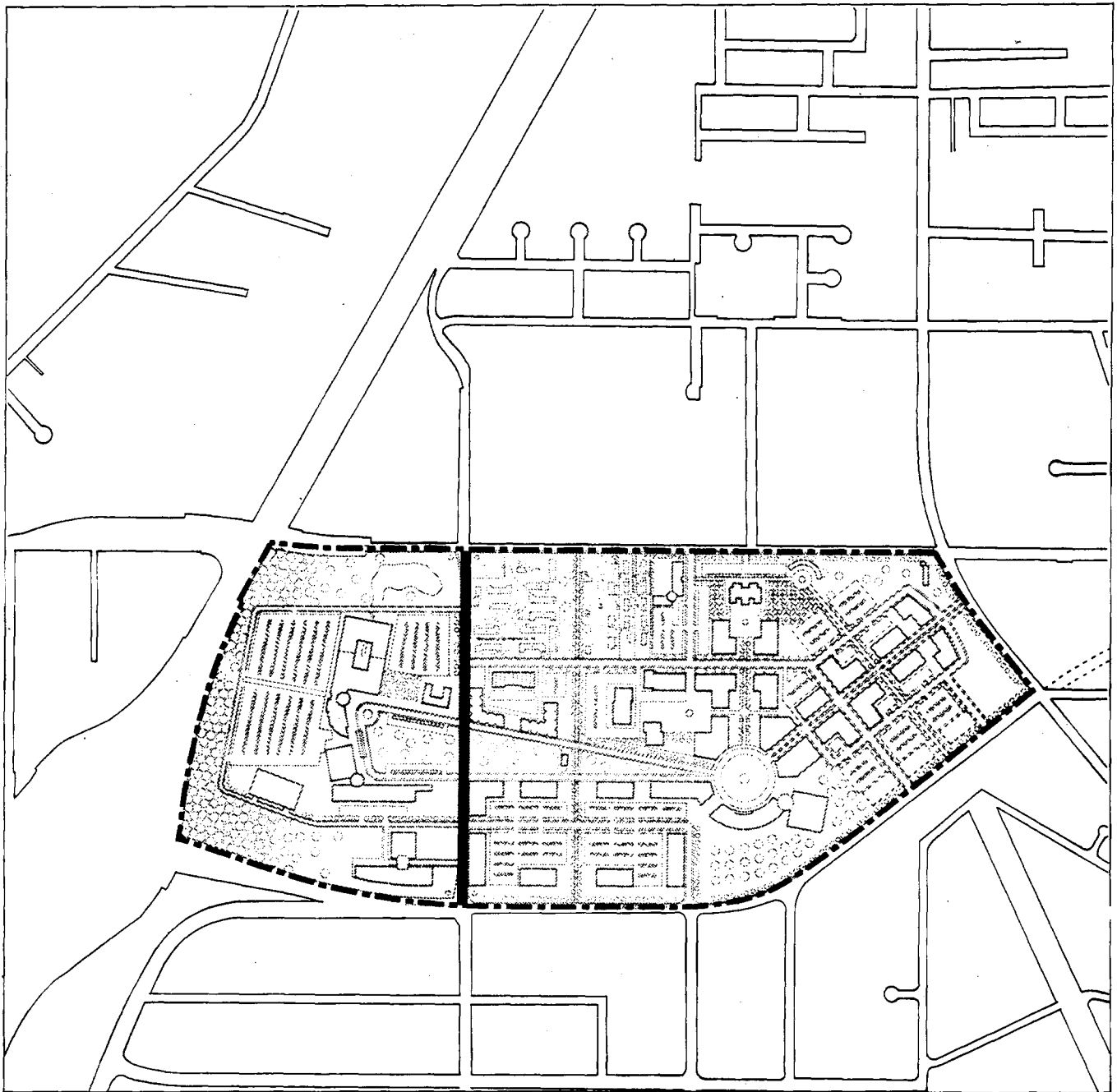
THE BOULEVARD

The orientation of the *Boulevard* encourages the symbolic linkage between state, city, and private uses. The *Boulevard* directly connects with Capitol Boulevard and extends west as *Main Street*, around the *Circle*, and along the edges of the *Triangle*. The *Boulevard* contains an open space that divides travel in the two directions. The *Boulevard's* open space varies in dimension to accommodate the lineal, tree-lined green of *Main Street* and the open spaces of the *Circle* and *Triangle*. As shown in the *Boulevard* graphic, the cross-section of the *Boulevard* contains a driving lane, an HOV lane, a dedicated bike lane, and a wide sidewalk in each direction. The HOV lane would accommodate transit vehicles, while the sidewalk would provide for varied pedestrian activities as well as a range of plantings and furniture. Sidewalk trees should be spaced no more than fifteen feet apart and should occur on both sides of the street.

Large deciduous trees should line the *Boulevard*, with a maximum setback from the curb of five feet, with maximum spacing of 15 feet. Trees along the western two-thirds of the *Triangle* should be randomly spaced. All streetside trees should be pollution-resistant and salt-tolerant.

The median should be no more than three feet high, including earth works and vegetation, but not including large deciduous trees, which are required on the median. The large trees shall be spaced no more than 15 feet on-center in the median.

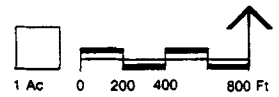
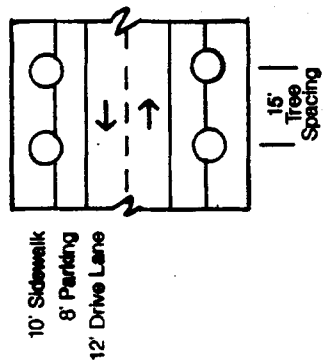
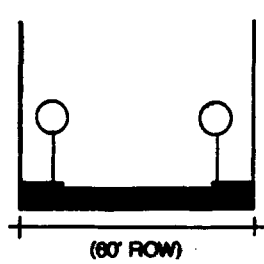
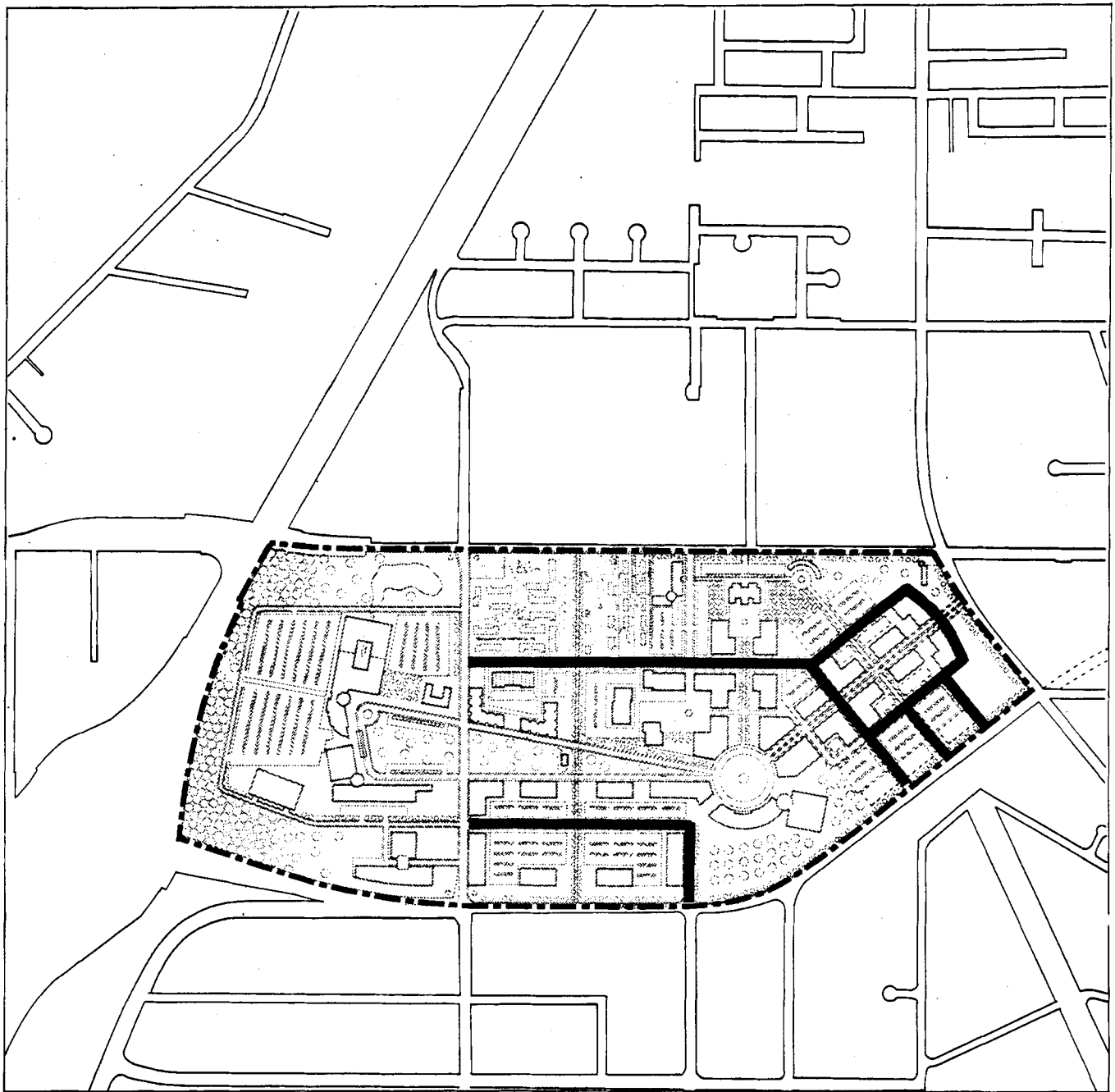
Street furniture, including benches, lighting, trash receptacles, hydrants, tree grates should reflect the civic qualities of this area, and its design should also reference the history of Tumwater.



STREET A

Street A (Linderson Way) permits north and south vehicular and pedestrian traffic through the site. As the only direct north and south Campus throughway, *Street A* provides two wide driving lanes in each direction and relatively wide sidewalks to promote through traffic. The emphasis of *Street A* should deter the movement of north and south traffic through other Campus pathways, limiting traffic northward to the existing residential neighborhood.

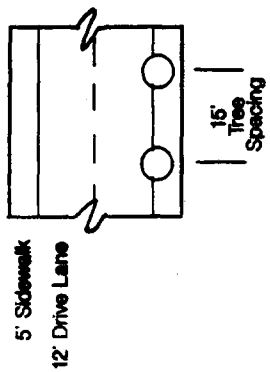
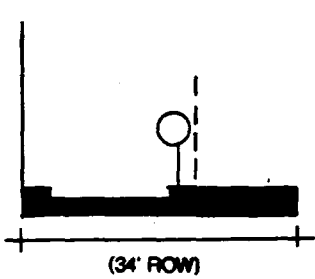
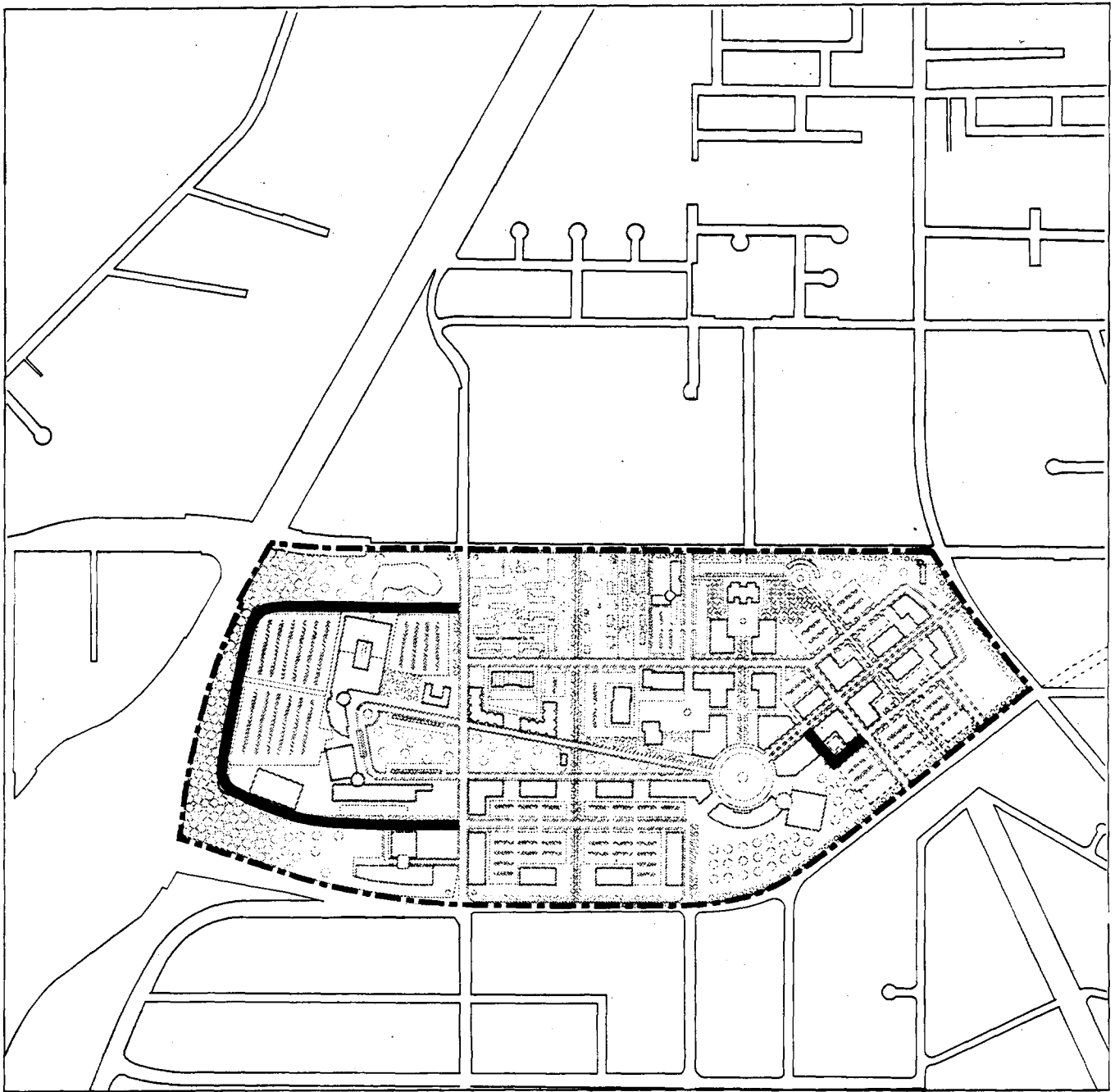
Sidewalk trees should be spaced no more than 15 feet apart and should occur on both sides of the street.



STREET B

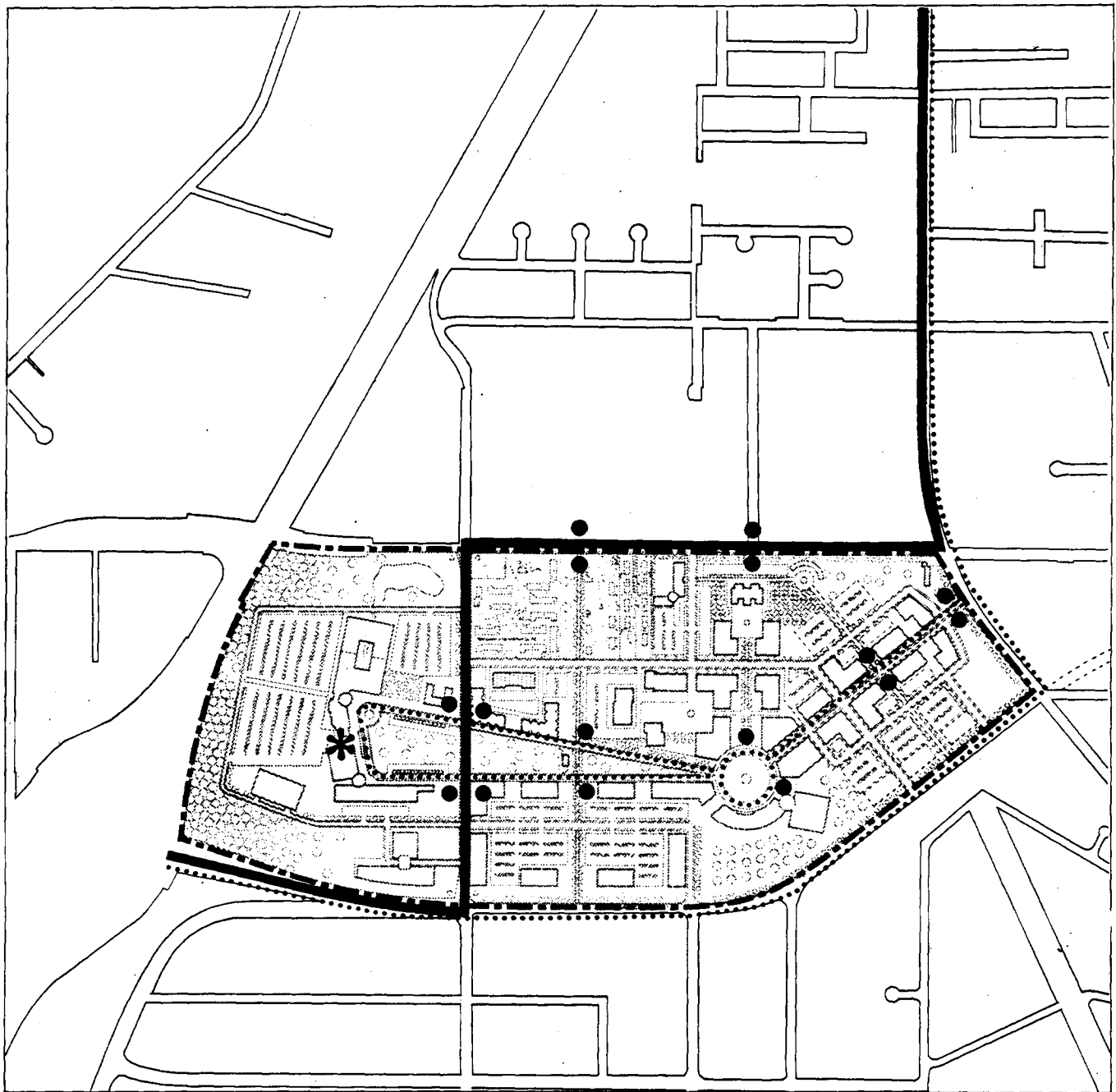
Street B provides a basic street for the Campus: two driving lanes, parallel on-street parking, and wide sidewalks. In Campus areas of retail and commercial use, *Street B* enables convenient short-term parking and direct pedestrian access.

Sidewalk trees should be spaced no more than 15 feet apart and should occur on both sides of the street.



ACCESS STREET

Access streets provide secondary vehicular and pedestrian circulation means to-and-from state facilities and associated parking structures and lots. This road type contains two wide lanes and modest sidewalks.



- Bus
- Van
- Stop
- * Transit Center



In combination with the bicycle and pedestrian pathways, the transit system provisions within the Campus intend to reduce the need for cars and other single-occupancy vehicles. Primary transit vehicle types would be vans and buses. As shown in the Roads section of these guidelines, transit vehicles travelling along the *Boulevard* would use the dedicated HOV lane.

Transit stops would be distributed every one to two Campus blocks as suggested by the graphic. At key transit stops, shelters should be provided. A "transit center" will be located at the point of highest user need. A likely location for this would be at the west end of the *Triangle*, fronting the state facilities. The transit center would comprise an open-air structure to accommodate a significant number of users and dedicated roadway to provide lineal space for three standing vehicles. All transit stops would provide clear signage and appropriate furniture and lighting.

In general, the range of transit vehicles and routes should provide a convenient means for travelling from and to the Olympia Campus, for destinations from Interstate 5, existing and future Tumwater area routes, and within the Campus itself. All transit-related designs must be coordinated with Intercity Transit. The Campus is one part of a regional transit system. The specific transit routes and stops must be reviewed by Intercity Transit, who will ultimately provide the service.

IMPLEMENTATION PLAN

The campus plan described here has the active support of the "partners" who helped craft it and will be responsible for its implementation:

- ▶ The State of Washington's Department of General Administration
- ▶ The City of Tumwater
- ▶ The Port of Olympia
- ▶ The New Market Skills Center/Tumwater School District
- ▶ Intercity Transit

Continued public participation will also be critical to help shape the plan's implementation.

Implementation of the Plan will begin with its formal adoption by the appropriate bodies within each of the partner organizations. That adoption process is now underway. Resolutions and letters of support have already been passed by the key participants.

For the State, adoption of the Plan comes with its approval by the State Capitol Committee on the recommendation of the Capital Campus Design Advisory Committee.

For the City of Tumwater, formal adoption involves City Council approval of the land uses and design guidelines specified in the Plan. Amendments to the comprehensive plan and zoning ordinance may occur.

For the Port of Olympia, adoption involves incorporation of the Plan into its Master Plans for the Thurston Airdustrial Center and Olympia Airport based on the binding development plan of project sequencing, and agency responsibilities documented in the Final Supplemental Environmental Impact Statement [FSEIS] for the campus.

Endorsement by the New Market Skills Center and the Tumwater School District involves each agency's board formally endorsing the Plan.